



CITY OF COCONUT CREEK

DEVELOPMENT REVIEW COMMITTEE (DRC) REVIEW #6 – SEPTEMBER 24, 2024

PROJECT NAME:	MainStreet Roadways		
PROJECT NUMBER:	PZ-22120002		
LOCATION:	Cullum Rd and Banks Road		
APPLICANT/AGENT:	HSQ Group		
REVIEW/APPLICATION:	PMDD Site Plan		
DISCIPLINE	REVIEWER	EMAIL	TELEPHONE
DRC Chair Urban Design & Development	Liz Aguiar - Principal Planner	laguiar@coconutcreek.net	(954) 973-6756
Sustainability, Urban Design & Photometrics	Linda Whitman – Sustainability Manager	lwhitman@coconutcreek.net	(954) 973-6756
Urban Design & Signage	Natacha Josiah - Planner	njosiah@coconutcreek.net	(954) 973-6756
Transportation	Michael Righetti - Senior Project Manager	mrighetti@coconutcreek.net	(954) 973-6756
Building	Sean Flanagan – Chief Building Official	sflanagan@coconutcreek.net	(954) 973-6750
Engineering	Eileen Cabrera - Senior Engineer	ecabrera@coconutcreek.net	(954) 973-6786
Fire	Ryan Banyas – Fire Marshal	rbanyas@coconutcreek.net	(954) 956-1563
Landscape	Scott Peavler - Landscape (consultant)	speavler@craventhompson.com	(954) 739-6400
Police	Barbara Hendrickx - Police Department	bhendrickx@coconutcreek.net	(954) 956-1474
ALTERNATE REVIEWERS			
Engineering	Mohammed Albassam- Engineer I	malbassam@coconutcreek.net	(954) 973-6786
Engineering	Thamar Joseph - Engineer I	tjoseph@coconutcreek.net	(954) 973-6786
Engineering	Steve Seegobin - Construction Supervisor	gseegobin@coconutcreek.net	(954) 973-6786

DEPARTMENT COMMENTS

BUILDING

This review shall not imply full compliance with the Florida Building Code. Submittal of a building permit application and plans are required for full review to obtain a building permit.

Note: Every effort has been made to identify code violations. Any oversight by the reviewer shall not be considered as authority to violate, set aside, cancel or alter applicable codes or ordinances. The plan review and permit issuance shall not be considered a warranty or guarantee. The designer is responsible for following all applicable federal, state, and municipal codes and ordinances.

Applicant is required to address **EACH** comment and to revise plans accordingly (*acknowledgements are not corrections*). **ONLY COMPLETE SIGNED AND SEALED DIGITAL PACKAGES WILL BE ACCEPTED.** Applicant does not need to resubmit application or previously submitted documents. Additional comments may be provided at DRC meeting and/or required upon review of any revised plans. Refer to **e-Plan User Guide** for instructions, found under resources on the Development Review web page.



ENGINEERING

Comment Type Legend:

- **Tier 1: Comment needs to be addressed at the DRC stage.**
- **Tier 2: Comment needs to be addressed at the Final Engineering Stage.**
- **Tier 3: Comment needs to be addressed at the Engineering Permitting Stage.**
- **Tier 2 and Tier 3 comments will remain as conditions of approval at the DRC level.**

General comments:

1. **(Tier 2)** Please note, all comments not addressed during the DRC review must be addressed during the final engineering review.
2. **(Tier 2)** Please note, all reviews and comments are preliminary. All plans are subject to further review for compliance with the City's Code of Ordinances, Utilities and Engineering Standards Manual, and Mainstreet Design Standards upon any site plan resubmittal and at final engineering review.
3. **(Tier 3)** All required approvals from FDOT, FDEP, Broward County, COCOMAR or any other applicable agencies must be obtained and submitted to the Engineering Division prior to issuance of an Engineering permits.
4. **(Tier 3)** In accordance with the City's code Sec.13-186, the execution of Performance Bonds for all proposed public improvements are required prior to issuance of an Engineering Permit.
5. **(Tier 2)** Provide reclaimed water and irrigation plans.
6. **(Tier 3)** Per City's code Sec.13-273, shop drawings for all proposed Water, Wastewater, Reclaimed water, and Drainage must be submitted prior to issuance of an engineering permit.
7. **(Tier 2)** Provide accurate station and offset numbers throughout all sheets.
8. **(Tier 2)** Please specify the number and size of the proposed conduit banks under the sidewalks. Coordination with communication companies is required to ensure size, location, and number of conduits are adequate for their needs.
9. **(Tier 2)** All proposed conduit banks must be placed below all proposed City utilities.
10. **(Tier 2)** During the roadway construction, all proposed utilities within the roadways must be installed and extended with stubs up to each block limit. The proposed utilities must connect to the adjacent parcels to ensure the system is looped.
11. **(Tier 2)** Provide irrigation plans and booster pump station locations with filtration systems.
12. **(Tier 2)** Ensure all proposed sidewalks comply with ADA standards, including sidewalks containing mast arms.
13. **(Tier 2)** Provided cross sections still show inconsistencies from plans. Provide consistent submittal.
14. **(Tier 2)** City staff has numerous comments on utilities, which are in conflict with the roadway plans. Additional review will be required based on proposed master utility plans.
15. **(Tier 2)** Ensure that all proposed utility locations shown on the cross-sections match what is on the site plan locations.

Applicant is required to address **EACH** comment and to revise plans accordingly (*acknowledgements are not corrections*). **ONLY COMPLETE SIGNED AND SEALED DIGITAL PACKAGES WILL BE ACCEPTED.** Applicant does not need to resubmit application or previously submitted documents. Additional comments may be provided at DRC meeting and/or required upon review of any revised plans. Refer to **e-Plan User Guide** for instructions, found under resources on the Development Review web page.



16. **(Tier 2)** Landscape, lighting, and/or irrigation shall not conflict with and shall maintain 10 feet preferred (minimum 6 feet) of separation from existing and proposed water, wastewater, and drainage.
17. **(Tier 3)** Please note, all Utilities & Engineering standard details have been revised. Engineering staff will provide a copy of the latest revisions. All standard details are subject to change until they are approved and codified.
18. **(Tier 2)** Include the milling and resurfacing of NW 40th Street as part of the City's cost share portion.
19. **(Tier 2)** Ensure that all parallel ADA parking spaces meet ADA standards for access aisle.
20. **(Tier 3)** Roadways abutting county/state R.O.Ws must obtain approval from corresponding agency.
21. **(Tier 2)** Previous comment has not been addressed: For all cross sections, specifically annotate **limerock** base for all LBR 100.
22. **(Tier 2)** Proposed forcemains are required to be epoxy coated DIP. Revise all annotations and cross sections accordingly. Some annotations still call HDPE.
23. **(Tier 2)** Remove all annotations for HSQ Group LLC, alignment.
24. **(Tier 2)** Provide the standard detail for the Paveway Treatment System installation.
25. **(Tier 2)** All proposed parallel ADA parking landing areas need to be striped accordingly.
26. **(Tier 2)** Ensure a minimum of 4 feet of separation is met between stop bars and crosswalks. Add note to plans.
27. **(Tier 2)** All proposed water taps must be double valved. Proposed plans call for single valves.
28. **(Tier 2)** Provide pavement restoration details per the City of Coconut Creek's Standard Details.
29. **(Tier 2)** Provide details for removable bollards.
30. **(Tier 2)** For all proposed pavers, distinguish the hatching between permeable and non-permeable pavers.
31. **(Tier 2)** Please provide additional elevations by the Final Engineering Review stage for all proposed roadways and drainage systems.
32. **(Tier 2)** Clarify different hatching symbols that are not shown on the legend for drainage pipes, for example along Cullum Road.
33. **(Tier 2)** For all proposed 10'x10' utility equipment easements, please ensure that there aren't any trees encroaching the proposed easement.
34. **(Tier 2)** Reclaimed water fire hydrants are not shown anywhere within the layout. They must be shown with the underground utility plan.
35. **(Tier 2)** It is recommended to have the reclaimed water main stub "off-set" to the back of F-curb, or installed parallel to the forcemain (per F.A.C. statutes) for future connection. By extending the (reclaimed water main) RWM to SR-7, this will yield any future impacts to the roadway.
36. **(Tier 2)** For all proposed reclaimed water main dead ends, propose a corresponding blow off assembly or automatic flusher.
37. **(Tier 2)** Advance signage must be installed for all midblock crosswalks and shown on the pavement marking plans.

Applicant is required to address **EACH** comment and to revise plans accordingly (*acknowledgements are not corrections*). **ONLY COMPLETE SIGNED AND SEALED DIGITAL PACKAGES WILL BE ACCEPTED.** Applicant does not need to resubmit application or previously submitted documents. Additional comments may be provided at DRC meeting and/or required upon review of any revised plans. Refer to **e-Plan User Guide** for instructions, found under resources on the Development Review web page.



38. **(Tier 2)** Drainage structures must not be in conflict with the proposed pavement markings.

City Market Ave:

39. **(Tier 2)** The proposed multi-use path along City Market Avenue needs to be at least 8' (feet) not 5' (feet) as proposed.

NW 40th Street:

40. **(Tier 2)** Sheet SP-19: Further review will be required for the note about the proposed ramps and crosswalks by others.

41. **(Tier 2)** Ensure that all proposed trees in the medians maintain appropriate separations from proposed City utilities.

42. **(Tier 2)** Sheet CS A4-A4: N.W.40th St west of Lyons Road - Monument sign within the median does not show any footer or applicable conduits, lighting etc. In addition, irrigation piping are not shown.

Banks Road:

43. **(Tier 2)** Sheet SP-10: As discussed, please analyze the design of the bike lane at the Sample Road intersection to better protect cyclist.

44. **(Tier 2)** Sheet SP-13: Previous comment not addressed: Include an item in the legend to identify the material used for the pattern shown for the FPL Maintenance Road.

Cullum Road:

45. **(Tier 2)** Sheets SP-1 and SP-2: Add the annotation and hatching layer for the milling and resurfacing of the entire lane on Cullum Road by the Developer. Please note, roadway restoration required for the forcemain must include milling and resurfacing of entire lane width. The remaining lanes, not affected by the forcemain installation will be the City's responsibility.

46. **(Tier 2)** Comment not addressed: The existing reclaimed water main line along the north side of Cullum Road is to be included within the cross sections.

47. **(Tier 2)** Proposed traffic calming devices must be coordinated with the proposed driveway for the new fire station on Block 14.

48. **(Tier 2)** Clarification is required for all of the F-curb inlets catch-basins connectivity along the north side of Cullum Road. Proposed drainage structures and the existing drainage manholes will be in conflict due to the their spread footer design.

NW 54th Terrace:

49. **(Tier 2)** Sheet SP-32: The proposed cross walk by Village Green along NW 54th Terrace needs to be shifted west so it can be aligned with all proposed cross walks locations.

50. **(Tier 2)** Sheet SP-32: SP 32 does not depict D-1, but does incorrectly depict D-2, and SP-33 incorrectly depicts D-3. Additionally, no cross section has been provided for NW 54th Terrace.

51. **(Tier 2)** Sheet SP-32: Comment has not been addressed: Drainage layout and profile does not match the cross section. The proposed drainage line and associated catch basins appear to be in the middle of the crowned road, whereas the road design cross section shows curb and gutter drainage.

52. **(Tier 2)** Sheet SP-33: Comment has not been addressed: One of the two existing City's forcemains is still not shown. Please refer to City's GIS system to locate all existing missing City utilities.



53. (Tier 2) Sheet LP-34: Comment has not been addressed: A tree is still proposed within the 15' utility easement on the west side of sheet. Please relocate.

NW 54th Ave:

54. (Tier 2) Sheet SP-34: Staff suggest a bike lane transition from the roadway to the multiuse path to be proposed at the intersection, not midblock.

FIRE

1. Prior to the issuance of a City of Coconut Creek Engineering permit for roadways shown on this site plan, the following comments shall be satisfied:
 - a. The following intersections shall have a corner radii sufficient enough to prevent encroachment of the fire apparatus into opposing traffic lanes:
 - Cullum Rd and NW 54th Ave
 - Cullum Rd and Banks Rd
 - Cullum Rd and NW 48th Ave
 - NW 40th St and Banks Rd
 - NW 40th St and City Market Ave
 - NW 40th St and NW 48th Ave
 - b. The following intersections shall have mountable curbs to allow for acceptable fire truck turning therefore preventing encroachment of the fire apparatus into opposing traffic lanes:
 - Cullum Rd and NW 54th Ave
 - Cullum Rd and Banks Rd
 - Cullum Rd and NW 48th Ave
 - NW 40th St and Banks Rd
 - NW 40th St and City Market Ave
 - NW 40th St and NW 48th Ave
 - c. The exact location and design of the traffic calming devices on Cullum Road may be modified, deleted, or relocated in order to accommodate the future Fire Station #113 driveway apron or preemption signal per study recommendations for Block 14, and to ensure that emergency response vehicles can travel in a safe and timely manner without damage to the vehicles or the median infrastructure.

LANDSCAPING

1. General Note: Tree Disposition plans and mitigation will be required to be approved prior to the issuance of a building or engineering permit for the individual blocks, roadways, and greenway/parks submittals. Comment remains pending approval of tree disposition and mitigation plans.
2. Sheet LP-39, Table 1, C1- Monarch High School ROW, correct provided tree and shrub/groundcover totals to meet the required quantity.
3. Sheet LP-6, plant label under key map is cut off by viewport.
4. Sheet LP-6, label relocated specimen Banyan Tree and provide tree ID #.

PHOTOMETRICS

No comments at this time.

Applicant is required to address **EACH** comment and to revise plans accordingly (*acknowledgements are not corrections*). **ONLY COMPLETE SIGNED AND SEALED DIGITAL PACKAGES WILL BE ACCEPTED.** Applicant does not need to resubmit application or previously submitted documents. Additional comments may be provided at DRC meeting and/or required upon review of any revised plans. Refer to **e-Plan User Guide** for instructions, found under resources on the Development Review web page.



POLICE

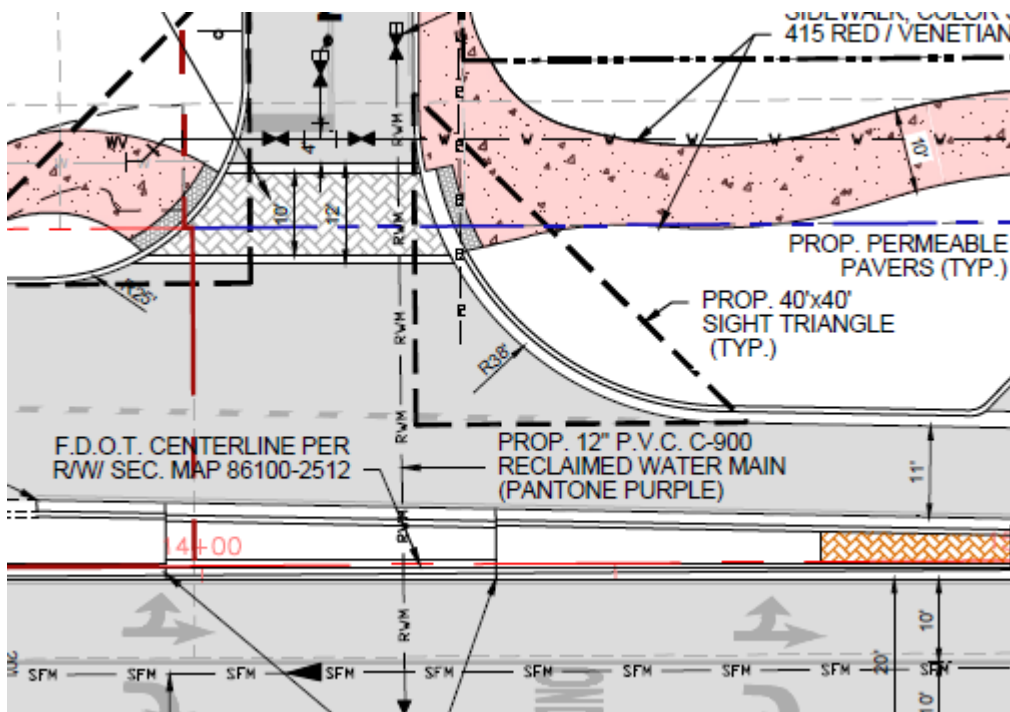
No comments at this time.

SUSTAINABILITY

No comments at this time.

TRANSPORTATION

2. Prior to the issuance of a City of Coconut Creek Engineering permit for roadways shown on this site plan, the following comments shall be satisfied:
 - a. City Staff appreciates the opportunity to have a “bike path” included on the southern segment of City Market Avenue. However, a 5-foot bike path is not consistent with other pathways within MainStreet. Refer to Sheet 058-SP-28 MainStreet City Market Avenue, where the easternmost “proposed bike path” shall be a minimum of 8-feet in width and the design team shall remove the western “proposed 5-foot bike path” provided that a concrete 8-foot connection to the Monarch Station Shared Use Pathway is included;
 - b. The Master Roadway PMDD Site Plan submittal reflects the proposed curb design. As per the virtual meeting with the design team, a mountable curb was to be incorporated into the intersections internal to MainStreet. The plan sheets appear to reflect only a D-curb. Please note on the plans where the two (2) different curbs segments begin and end at or near the intersections;



- c. Provide a typical cross-section of Cullum Road segment where the traffic calming medians are located. Provide dimensions and horizontal control that illustrates curb & gutter, travel lane widths, sidewalk widths, etc. (as a reduction of the median width may be required);

Applicant is required to address **EACH** comment and to revise plans accordingly (*acknowledgements are not corrections*). **ONLY COMPLETE SIGNED AND SEALED DIGITAL PACKAGES WILL BE ACCEPTED.** Applicant does not need to resubmit application or previously submitted documents. Additional comments may be provided at DRC meeting and/or required upon review of any revised plans. Refer to **e-Plan User Guide** for instructions, found under resources on the Development Review web page.



- d. As discussed during our recent virtual meeting, please provide a note on the Roadways Site Plans that reflects “The location of the Cullum Road medians may be adjusted at the direction of the City Engineer, City Manager or their designee to accommodate the future access driveways for Fire Station 113 on Block 14”;
- e. Cullum Road construction shall be completed as part of the requirements outlined by the MAP Broward (Surtax) guidelines and/or provisions or conditions noted within the Project Specific Interlocal Agreement for Cullum Road;
- f. The Fire Truck Routing Plan does not show a truck at the intersection of NW 54th Avenue & Cullum Road (northbound to eastbound);
- g. The use of mountable curbs may be used to allow for fire truck access at all internal intersections (review Fire Truck access conditions at City Market Avenue & NW 40th Street); and
- h. Review and update the Fire Access Route at NW 40th Street and Banks Road. City Staff generally does not support Fire Truck turning movements that encroach into on-coming traffic at the end of the turn.

URBAN DESIGN AND DEVELOPMENT

1. Roadways plan is subject to City Commission approval.
2. Pending modifications to plans per outstanding DRC comments. Corrections must be resubmitted to the Urban Design Division for staff review prior to the issuance of a building permit or as otherwise stated.
3. Additional comments may be provided upon submittal and review of any revised plans.
4. Pending reimbursement to the City for professional review services prior to building permit issuance.
5. Pending recorded public access easements.
6. Pending plat recordation.
7. Pending applicant PowerPoint presentation at public meetings.
8. Pending receipt of 1 digital and 13 printed application packages as previously discussed.
9. Pending submittal of proposed Amenities/Imaging Package including street signage for review and comment.
10. Pending parks plan review and utilities conflicts resolution within islands / landscaped areas.
11. Be advised, individual Block ground signs are not approved as shown on roadways plans. Signage requires separate review and approval prior to building permit issuance.
12. Where applicable, ensure crosswalks include typical paver treatment system as required. Ensure ALL plans are updated to reflect this condition.
13. Sheets SP-2 & SP-3: The existing headwall to remain at the west edge of the buffer adjacent to Block 9 leaves a portion of the ditch open. Provide a “cloud” around this area and include a note on the plan committing to cooperating with the City at a future date to remove the headwall, fill in the remaining open ditch and extend the greenway design on the east over the remaining open ditch area. The City wants an assurance that the future Property Association or HOA will not prevent the

Applicant is required to address **EACH** comment and to revise plans accordingly (*acknowledgements are not corrections*). **ONLY COMPLETE SIGNED AND SEALED DIGITAL PACKAGES WILL BE ACCEPTED.** Applicant does not need to resubmit application or previously submitted documents. Additional comments may be provided at DRC meeting and/or required upon review of any revised plans. Refer to **e-Plan User Guide** for instructions, found under resources on the Development Review web page.



City from completing this project. Also, provide a written acknowledgement and commitment in DRC response as referenced herein.

14. Sheet SP-4: May require modification based on final Block 10 design.
15. Sheet LP-27: SW corner of Cullum Road and NW 54th Avenue is an important focal point. Revise plan to provide pedestrian or other amenity that is consistent with the restrictions in the Cocomar Easement. Ground signage may not be an enhancement that Cocomar would permit in the easement.
16. Sheet LP-35: Ensure there are no landscape conflicts with fire equipment on the east side of trash enclosure on Block 15B.
17. All required and pending resubmittals including cross sections and other details shall be submitted for staff review and approval prior to the issuance of an engineering permit.



September 12, 2024

CITY OF COCONUT CREEK
4800 West Copans Road
Coconut Creek, FL 33063
Attn: Liz Aguiar

Project Name: MainStreet Roadways
Project Number: PZ-22120002
Location: Cullum Rd and Banks Road
Applicant/Agent: HSQ Group
Review/Application: PMDD Site Plan

Ms. Aguiar,

We have reviewed the DRC #5 Comments request dated August 22, 2024, and our responses are as follows:

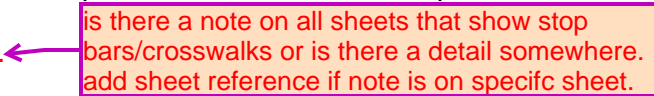
ENGINEERING

General comments:

- (Tier 2)** Please note, all comments not addressed during the DRC review must be addressed during the final engineering review.
RESPONSE: The final construction plans will incorporate any comments not addressed on the conceptual plans.
- (Tier 2)** Please note, all reviews and comments are preliminary and plans are subject to further review for compliance with the City's Code of Ordinances, Utilities and Engineering Standards Manual, and Mainstreet Design Standards upon any site plan resubmittal and at final engineering review.
RESPONSE: The final construction plans will meet all city codes and ordinances.
- (Tier 3)** All required approvals from FDOT, FDEP, Broward County, COCOMAR or any other applicable agencies must be obtained and submitted to the Engineering Division prior to issuance of Engineering permits.
RESPONSE: All required government agency approvals will be obtained prior to construction commencement.
- (Tier 3)** In accordance with the City's code Sec.13-186, the execution of Performance Bonds for all proposed public improvements are required prior to issuance of Engineering Permit.
RESPONSE: The bonds will be posted during the permitting process.
- (Tier 2)** Please provide reclaimed water and irrigation plans.
RESPONSE: Plans will be provided during the permit process.

6. **(Tier 3)** Per City code Sec.13-273, shop drawings for all proposed Water, Wastewater, Reclaimed water, and Drainage must be submitted prior to issuance of engineering permit.
RESPONSE: Shop drawings will be provided during the permitting process.
7. **(Tier 2)** Provide accurate station and offset numbers throughout all sheets.
RESPONSE: The final civil plans will include station and offsite.
8. **(Tier 2)** Specify the number and size of the proposed conduit banks under the sidewalks, coordination with communication companies is required to ensure size, location, and number of conduits are adequate for their needs.
RESPONSE: The conduit final design information will be provided once we complete our coordination with the utility companies.
9. **(Tier 2)** All proposed conduit banks must be placed below all proposed City utilities.
RESPONSE: The conduits are generally below the water main, force main and re-use mains, but may be above deeper utilities to include drainage pipes and gravity sewer pipes.

check - Sheet RD cross sections seem to show conduits above the re-use main and other utilities
10. **(Tier 2)** During the roadway construction, all proposed utilities within the roadways must be installed and extended with stubs up to each block limit. The proposed utilities must connect to the adjacent parcels to ensure the system is looped.
RESPONSE: Comment acknowledged. The phasing of construction is part of the coordination with the city.
11. **(Tier 2)** Provide irrigation plans and booster pump station locations with filtration systems.
RESPONSE: Irrigation plans will be provided at the permitting stage.
12. **(Tier 2)** Ensure all proposed sidewalks comply with ADA standards, including sidewalks with mast arms.
RESPONSE: The final design will comply with ADA standards.
13. **(Tier 2)** Cross sections still show inconsistencies from the plans. Provide a consistent submittal.
RESPONSE: The cross sections have been updated to match the plans.
14. **(Tier 2)** City staff has numerous comments on the utilities, which are in conflict with the roadway plans. Additional review will be required based on proposed master utility plans.
RESPONSE: The final engineering plans will include conflict information
15. **(Tier 2)** Ensure all proposed utility locations shown on cross-sections match site plan locations.
RESPONSE: Final plans will include this information.
16. **(Tier 2)** Landscape, lighting, and/or irrigation shall not conflict with and shall maintain 10 feet preferred (minimum 6 feet) of separation from existing and proposed water, wastewater, and drainage.
RESPONSE: Plans will meet this criterion.

17. **(Tier 3)** Please be aware that all Utilities & Engineering standard details have been revised, and engineering staff will provide a copy of the latest revisions. All standard details are subject to change until they are approved and codified.
RESPONSE: Plan details have been updated with the latest standards.
18. **(Tier 2)** Include the milling and resurfacing of 40th Street as part of the City's cost share portion.
RESPONSE: The cost estimate will include the city's portion of the work.
19. **(Tier 2)** Ensure that all parallel ADA parking spaces meet ADA standards for access aisle.
RESPONSE: The design will meet ADA standards.
20. **(Tier 3)** Roadways abutting county/state R.O.Ws must obtain approval from corresponding agency.
RESPONSE: We will obtain the required government permits.
21. **(Tier 2)** Previous comment has not been addressed: For all cross sections, please specifically annotate limerock base for all LBR 100.
RESPONSE: The LBR 100 is noted on the detail sheets.
22. **(Tier 2)** Proposed force mains are required to be epoxy coated DIP. Please revise all annotations and cross sections accordingly. Some annotations still call HDPE.
RESPONSE: The force main call outs have been changed.
23. **(Tier 2)** Remove all annotations for HSQ Group LLC, alignment.
RESPONSE: This will be removed on the final plans.
24. **(Tier 2)** Provide the standard detail for the Paveway Treatment System installation.
RESPONSE: Details will be provided on the final engineering plans.
25. **(Tier 2)** All proposed parallel ADA parking landing areas need to be stripped accordingly.
RESPONSE: Additional pavement markings will be added on the final engineering plans.
26. **(Tier 2)** Please ensure a minimum of 4 feet of separation is met between stop bars and crosswalks. Note should be added on plans.
RESPONSE: A dimension is on the plans. 
27. **(Tier 2)** All proposed water taps must be double valved. Proposed plans call for single valves.
RESPONSE: The additional valve has been added to the plans.
28. **(Tier 2)** Provide pavement restoration details per the City of Coconut Creek's Standard Details.
RESPONSE: The city will provide us with the latest details for the restoration.
29. **(Tier 2)** Provide details for removable bollards.
RESPONSE: The details will be provided on the final engineering plans.

30. **(Tier 2)** For all proposed pavers, distinguish the hatching between permeable and non-permeable pavers.
RESPONSE: The final engineering plans will include the differences in paver types.
31. **(Tier 2)** Provide additional elevations by the Final Engineering Review stage for all proposed roadways and drainage systems.
RESPONSE: Additional elevations will be on the final engineering plans.
32. **(Tier 2)** Clarify different hatching symbols that are not shown on the legend for drainage pipes, for example along Cullum Rd.
RESPONSE: The large diameter pipes use a hatch on the pipe to show the accurate width of the pipe for conflict information.
33. **(Tier 2)** For all proposed 10x10 utility equipment easements, please ensure that there aren't any trees encroaching the proposed easement.
RESPONSE: This will be addressed on the final landscape plans.
34. **(Tier 2)** Reclaimed water fire hydrants are not shown anywhere within the layout. They must be shown with the underground utility plan.
RESPONSE: The reclaim hydrants are shown on the plans.
35. **(Tier 2)** It is recommended to have the reclaimed water main stub "off-set" to the back of F-curb, or installed parallel to the forcemain (per F.A.C. statutes) for future connection. By extending the (reclaimed water main) RWM to SR-7, this will yield any future impacts to the roadway.
RESPONSE: This will be addressed on final engineering plans.
36. **(Tier 2)** For all proposed reclaimed water main dead ends, propose a corresponding blow off assembly or automatic flusher.
RESPONSE: A blow off hydrant has been proposed for the dead ends.
37. **(Tier 2)** Advance signage must be installed for all midblock crosswalks and shown on the pavement marking plans.
RESPONSE: The flashing signs will be installed for mid-block pedestrian crossings.
38. **(Tier 2)** Drainage structures must not be in conflict with the proposed pavement markings.
RESPONSE: The pavement marking will skip over any drainage structures.

RWM plug & gate valve is shown on SP-2. DOES THIS ADDRESS THE COMMENT? INCLUDE SHEET REFERENCE IN RESPONSE.

City Market Ave:

39. **(Tier 1)** As previously discussed with staff, please add bike lane on City Market Ave.
RESPONSE: bike lanes have been added to both sides of the road at the R/W line. The existing sidewalk on Monarch station is utilized for pedestrian connectivity. The future developer of the Eastern parcel will modify the roadway based upon their future design.

NW 40th Street:

- 40. (Tier 2) Sheet SP-19: Further review will be required for the note about the proposed ramps and crosswalks by others.
RESPONSE: The sidewalks and ramps outside of the PMDD boundary are shown as requested by the city staff to allow the adjacent property owners to construct the improvements associated with their property to be consistent with the Mainstreet design standards.

- 41. (Tier 2) Ensure all proposed trees in the medians maintain appropriate separations from proposed City utilities.
RESPONSE: The trees will meet the required horizontal separation.

- 42. (Tier 2) CS A4-A4: N.W.40th St west of Lyons Rd- Monument sign within the median does not show any footer or applicable conduits, lighting etc. In addition, irrigation piping are not shown.
RESPONSE: Final design of the monument sign will be provided at building permit as a separate design document. The cross section only shows the approximate location for graphical purposes.

Banks Road:

- 43. (Tier 2) Sheet SP-10: As discussed, please analyze the design of the bike lane at the Sample Road intersection to better protect cyclist.
RESPONSE: Please see the revised plan with a channeled bike lane to protect the cyclist from vehicles making a right turn.

- 44. (Tier 2) Sheet SP-13: Previous comment not addressed. Include an item in the legend to identify the material used for the pattern shown for the FPL Maintenance Road.
RESPONSE: Please see sheets with the maintenance road with updated legend.

Cullum Road:

- 45. (Tier 2) Sheets SP-1 and SP-2: Add the annotation and hatching layer for the milling and resurfacing of the entire lane on Cullum Road by the Developer. Please note that roadway restoration required for the forcemain must include milling and resurfacing of entire lane width. The remaining lanes, not affected by the forcemain installation will be the City's responsibility.
RESPONSE: The final engineering plans will show separate hatches for the milling and resurfacing by the developer and by the city.

- 46. (Tier 2) Comment not addressed: The existing reclaimed water main line along the north side of Cullum Road is to be included within the cross sections.
RESPONSE: The reclaimed water main has been added to the cross section.

- 47. (Tier 2) Proposed traffic calming devices must be coordinated with the proposed driveway for the new fire station on Block 14.
RESPONSE: The traffic calming median has been moved East to avoid the future fire station driveway. The actual driveway location and size has not been given to us to show on the plans yet.

clarify - looking at SECTION C1, SP-1 AND SP-2 AND DON'T SEE RECLAIMED MAIN ON NORTH SIDE SHOWN. INDICATE WHICH PLAN SHEET THIS IS SHOWN

CLARIFY RESPONSE - WASN'T THE MEDIAN SHIFTED WEST (NOT EAST)...AND SP-6 SHOWS A DRIVEWAY TO BLOCK 14 AND NOTES "FINAL LOCATION PER CITY DIRECTION"

EDIT PLAN NOTE AT DRIVEWAY FOR CLARIFICATION?... "POTENTIAL ENTRANCE TO BLOCK 14 - FINAL LOCATION TO BE DETERMINED BY CITY)

48. **(Tier 2)** Clarification is required for all of the F-curb inlets catch-basins connectivity along the North side of Cullum Rd. Proposed drainage structures and the existing drainage manholes will be in conflict due to the their spread footer design.

RESPONSE: The final design will be provided as part of the final civil engineering plans at time of permitting.

CLARIFY THIS RESPONSE - NORTH OF THE CLUBHOUSE WITHIN CITY BLOCK 15C THE R/W LIMIT IS NOT CLEAR ON THE PLANS. SIDEWALK ADJ. TO WEST SIDE OF THE LAKE MIGHT BE OUTSIDE OF THE R/W (WITIN CITY OWNED GREENSPACE)

NW 54th Terrace:

49. **(Tier 1)** Sheet SP-32: The proposed sidewalk south of NW 54th Terrace needs to be continuous to the greenway adjacent to Lake 15A.

RESPONSE: Per our coordination with the city staff we modified the section to reduce the lanes to 10' wide for a total of 20' wide pavement with type D curb on the sides to make room for a 5' sidewalk along the East side of the road within the R/W limits. This provides for the sidewalk connectivity along NW 54th Ter from Cullum road to Wiles Road.

50. **(Tier 2)** Sheet SP-32: SP 32 does not depict D-1, but does incorrectly depict D-2, and SP-33 incorrectly depicts D-3. Additionally, no cross section has been provided for NW 54th Terrace.

RESPONSE: The cross sections have been revised and additional cross sections have been provided for NW 54th Terrace.

ADD: Please refer to sheet RD-4, Sections F-1, F-2 and F-3 for NW 54th Terr. and Sheet SP-32 with updated section references. Section D-1, D-2, D-3 & D-4 on Sheet RD-3 can be found on NW 48th Avenue.

51. **(Tier 2)** Sheet SP-32: Comment not addressed: Drainage layout and profile does not match cross section. Proposed drainage line and associated catch basins appear to be in the middle of the crowned road, whereas the road design cross section shows curb and gutter drainage.

RESPONSE: The cross sections numbering has been updated to the correct section for NW 54th Terrace.

52. **(Tier 2)** Sheet SP-33: Comment not addressed: One of the two existing City's force mains is still not shown, please refer to City's GIS system to locate all existing missing City utilities.

RESPONSE: we will review the GIS and update the plans.

53. **(Tier 2)** Sheet LP-34: Comment not addressed: A tree is still proposed within the 15' utility easement on the west side of sheet, please relocate.

RESPONSE: The tree has been moved out of the 15' easement.

NW 54th Ave:

54. **(Tier 2)** Sheet SP-34: Staff suggest bike lane transition from the roadway to the multiuse path to be proposed at the intersection not midblock.

RESPONSE: The lane width does not allow enough room for the bike lane to make it to the intersection.

FIRE

1. The scale of the Fire Truck AND Other Utility/Solid Waste Vehicle/Truck Circulation Plans are not clear enough. Fire cannot determine the turning radius on some intersections.

RESPONSE: Smaller scale plans at 1"=40' was submitted to Fire for clarity.

WERE SUBMITTED DIRECTLY TO FIRE FOR CLARITY AND ARE INCLUDED WITH THIS RESUBMITTAL.

2. Due to the large 1"=200' scale on the Fire Truck Routing Plan, the illustration of the fire truck traversing traffic calming on Cullum Road is unclear. Clarify with a smaller scale of 1"=40' or 1"=20'.

RESPONSE: Plans at 1"=40' was submitted to fire for review.

WERE SUBMITTED DIRECTLY TO FIRE FOR CLARITY AND ARE INCLUDED WITH THIS RESUBMITTAL.

3. Pending coordination with city staff for the proposed locations of potential future driveways leading to Block 14 from Cullum Road. The driveway aprons on the conceptual site plan for Block 14 appear to be in conflict with the traffic calming on SP-6.

RESPONSE: The traffic calming median has been moved East to avoid the future driveway to the fire station.

CLARIFY RESPONSE - WASN'T THE MEDIAN SHIFTED WEST (NOT EAST)...

EDIT SP-6 PLAN NOTE AT DRIVEWAY FOR CLARIFICATION?... "POTENTIAL ENTRANCE TO BLOCK 14 - FINAL LOCATION TO BE DETERMINED BY CITY)

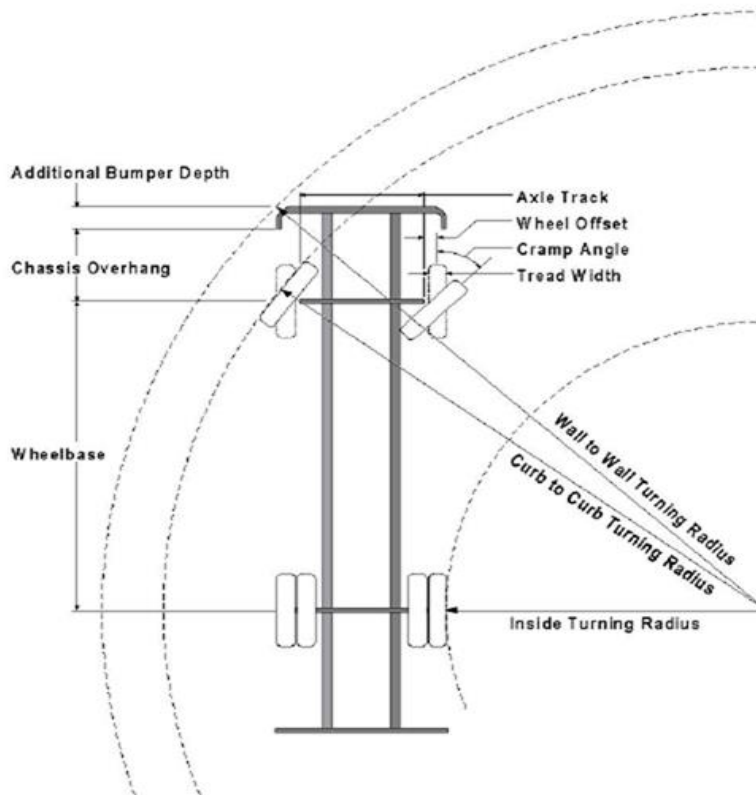
4. After recently receiving more detailed plans on some of the other blocks, the scale of 1"=200' used for the fire truck routing plan appears unsatisfactory. Provide a smaller scale (1"=40' or 1"=20') illustration of the intersections.

RESPONSE: Plans at 1"=40' scale have been submitted to the city.

WERE SUBMITTED DIRECTLY TO FIRE FOR CLARITY AND ARE INCLUDED WITH THIS RESUBMITTAL.

5. The Fire Truck Route Plan shall include a two axle fire truck (B40 Bus template) that is 39 feet long, 9 feet wide. The turning radius dimensions, front wheel path, rear wheel path, apparatus path, and front chassis overhang (see figure) shall be displayed. Use a 50 foot outside 25 foot inside turning radius. Do not overlap the entrance and exit of the truck and provide multiple pages if necessary. (NFPA 1-18.2.3.1.1).

RESPONSE: The fire truck plans have the 39' long by 9' wide truck. The lanes in most areas are 10' wide as required by the city which only leave one foot of clear area for the fire truck to maneuver. We used a 38' curb radius to allow the truck to make the right turns at intersection easier than a tighter 25' radius. Per our meeting with Fire we changed several intersection curb radii to 50' and provided a Type E mountable curb at the intersections in case the truck drives over the curb to avoid other vehicles during emergency situations.



LANDSCAPING

1. General Note: Tree Disposition plans and mitigation will be required to be approved prior to site plan approval for the individual blocks, roadways, and greenway / parks submittals. Comment remains pending approval of tree disposition and mitigation plans.

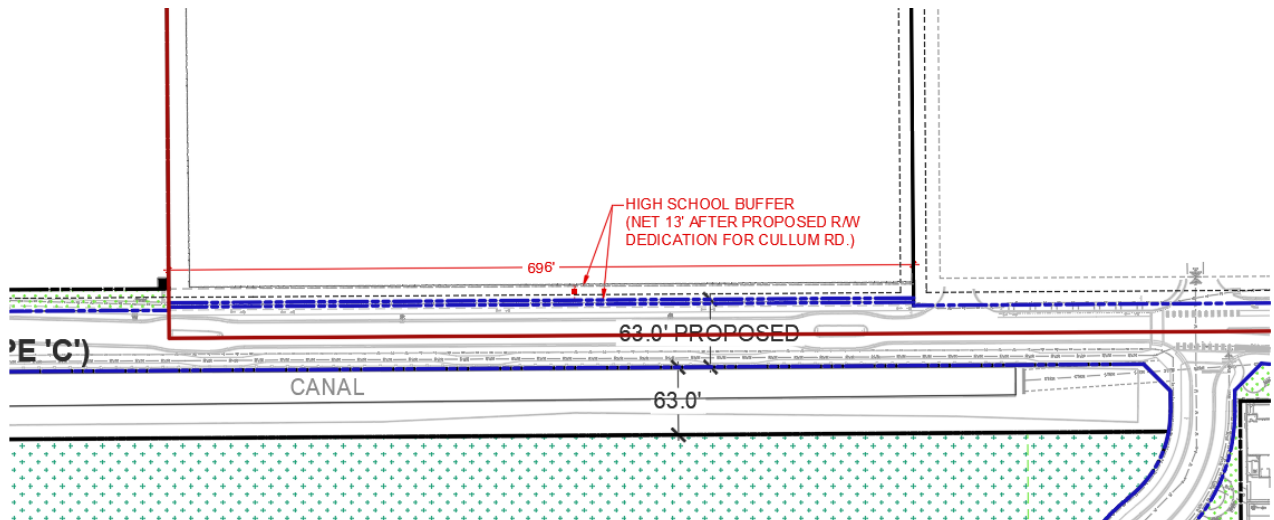
Response: Tree disposition plans with mitigation will be provided with the park plan resubmittal.

2. As per the meeting with planning staff, the data tables for the Streetscape requirements along Cullum Road are to be updated to separate out the specific landscape material associated with the adjacent City and School properties.

RESPONSE: Please see revised sheet LP-39 with updated data tables. Copied below are the requirements per the approved Mainstreet Design Standards for the north side of Cullum Road, within the adjacent School property.

Section C1 Planting Requirements	
Refer to LDR Section 13-444(c) for native plant & species variety requirements.	
Shrubs / Groundcover	
• Drainage Canal:	- 100% in sod
• North & South Side landscape strips:	- 40% Shrub / Groundcover Coverage. Balance in Sod.
Trees / Palms *	
• North & South Side landscape strips :	- One (1) shade or flowering tree per each thirty (30) linear feet.
	- Trees may be clustered. Spacing maximum 60' o.c.
	- Up to 50% of the required trees may be substituted with palms. Palms where utilized are counted as 3:1 with exception of specimen/formal palms are counted 1:1, per LDR Sec. 13-444(c)(1).
* North Side Palms & Trees to be provided within adjacent parcel buffer (High School Parcel) and adjacent parcel proposed greenway (City Civic Parcel).	
* South Side Palms & Trees subject to coordination with Cocomar Canal Maintenance Requirements and compliance with City required utility separations.	

The school buffer adjacent to Cullum Road is 696 linear feet X 13' width (after Cullum Road dedication of proposed 63' from centerline). 24 trees/palms are required (1/30 LF) and 3,619 shrubs/ground cover are required (40% of 9,048 SF strip) within the schools buffer.



3. Proposed tree quantity along NW 54th Terrace on sheets LP-34 and LP-35 are not consistent with total stated in landscape requirement table on sheet LP-39.
RESPONSE: Tree quantity has been updated to match the provided number on the NW 54th Terrace tabular data. The additional trees that we are providing along that road will go towards the Cullum Road landscape strip tree deficit in an effort to meet our requirements.
4. Sheet LP-36, as discussed with applicant LA, provide island tree on the east side of the south large center island along Village Square Road, as well as root barrier. In addition, per the meeting with planning staff the landscape islands along Village Square Road are to be reviewed by the applicant to determine if they can be adjusted to accommodate larger shade trees. This includes adjustment to sidewalks, underground utilities, and curbing.
RESPONSE: Utilities have been adjusted to accommodate a tree in the terminal island. Additionally, structural soil is being provided for the planting areas around the Village Green roadway to accommodate larger shade trees in the terminal islands and landscape strips. See sheets LP-36 and LP-37.
5. On Sheet LP-36 and LP-37, for street type B-3, parking islands are required to provide 1 shade tree per island. Applicant to submit a waiver request and justification for the tree category deviation. Comment not addressed; waiver request has not been submitted. This must be submitted and approved prior to the Planning and Zoning Board public hearing.
RESPONSE: Structural soil is being provided for the planting areas around the Village Green roadway to accommodate larger shade trees in the terminal islands and landscape strips. See sheets LP-36 and LP-37.
6. Street names at the top of the plant list columns are cut off. Adjust tables.
RESPONSE: Plant list has been updated to clearly show the street names, sheet numbers, and street types on sheet LP-38.
7. Provide relocated tree id #s on plan sheets LP-6 and LP-7 in addition to listing them in the plant schedule. Include large, relocated specimen tree id # as well.
RESPONSE: Tree disposition numbers have been provided for the relocated Bismark palms and the banyan tree shown on sheets LP-6, LP-7, and LP-8.
8. Provide required quantities for trees and coverage square footage under Cullum Road, Street Type C3 Median Requirements, Table 3.
RESPONSE: The required quantity amount has been added to the Street Type C1 Median requirements on sheet LP-39.
9. Plant schedule on sheets LP-34 through LP-37 are very light and not legible.
RESPONSE: Plant schedule has been added to the sheets.

PLS CONFIRM THIS WAS DONE. DON'T SEE RELOCATED ID #'S ON THE PLAN OR PLANT LEGEND ON THESE SHEETS

TRANSPORTATION

1. The MainStreet Blocks that are directly adjacent to Lyons Road may be required to dedicate and record, by separate instrument, a "Bus Shelter Easement" and/or "Greenway Easement" for the segments of the public improvements to identify the perpetual maintenance obligations associated with, but not limited to, the Lyons Road decorative pedestrian lighting, landscaping, bus rider shelters, public art, greenway segments, irrigation, etc.;
RESPONSE: The bus stop locations on Lyons road have been coordinated with Mass Transit.

ELABORATE THE RESPONSE OR SAY "ACKNOWLEDGE"? IT IS NOT JUST ABOUT BUS SHELTERS

Please refer to Sheet SP-10. ... (the sheet includes an image of "Bike Path Barrier" but not clearly noted on plan view where this is proposed)

2. Banks Road connection to Sample Road – the implementation of additional roadway materials (curb, bollards, etc.) into the design in order to plan and construct a true protected bike lane was not incorporated into the petitioner’s response, as the comment provided from staff was pretty much ignored.
RESPONSE: The bike path barriers were added to the plans which are raised barriers to protect the bicyclist. Please let us know if this is acceptable.

3. Staff’s response: The petitioner has committed to other MainStreet roadway improvements that are outside of the “project area” and these roadways are either being improved, designed and/or constructed;

- a. Southern portion of City Market Avenue that connects to Sample Road (not directly adjacent to MainStreet);
- b. Banks Road landscaping adjacent to Monarch High School (shown but noted as “to be completed by others”);
- c. WB right turn lane on Sample Road that connects to City Market Avenue (not directly adjacent to MainStreet);
- d. Southern portion of NW 54th Avenue (pavement markings);
- e. Repaving of Cullum Road from NW 54th Avenue all the way to SR7/441 (not directly adjacent to MainStreet);
- f. All of the roadways around Village Green; and
- g. EB Wiles Road right turn lane onto Banks Road;

RESPONSE: These improvements will be constructed. The shared costs with the city are per the developer’s agreement.

4. Moreover, City Staff secured over 6-million dollars to apply towards construction funding for Cullum Road and considering the DRI commitments to construct a signal at this intersection as well. The request outlined below to have the Design Team generate plans for a roundabout at NW 54th Avenue and the design of NW 40th Street from Banks Road to NW 54th Avenue, is recommended for consideration. The comment remains outstanding and should be incorporated into the Developer’s Agreement cost sharing spreadsheet, in order to promote further discussion and consideration.

RESPONSE: The additional city improvements could be constructed with the Mainstreet project.

5. The Typical Roadway Section C-1 provides for 11.5 foot travel lanes on Cullum Road. This should be revised to provide EB Cullum Road with 10-foot travel lanes adjacent to Block 9. The additional 3-feet can be added to either the on-street parking, greenway/landscaping and/or the Cullum Road median.

RESPONSE: The roadway adjacent to the east portion of Block 9 at the intersection to 54th Ave. has been revised to 10’ wide lanes and added to the south side landscape strip. This allows the thru lane to align with Cullum road East of the intersection. The lanes at the west portion of Block 9 continue to taper to align with the existing wider travel lanes to the west of Mainstreet.

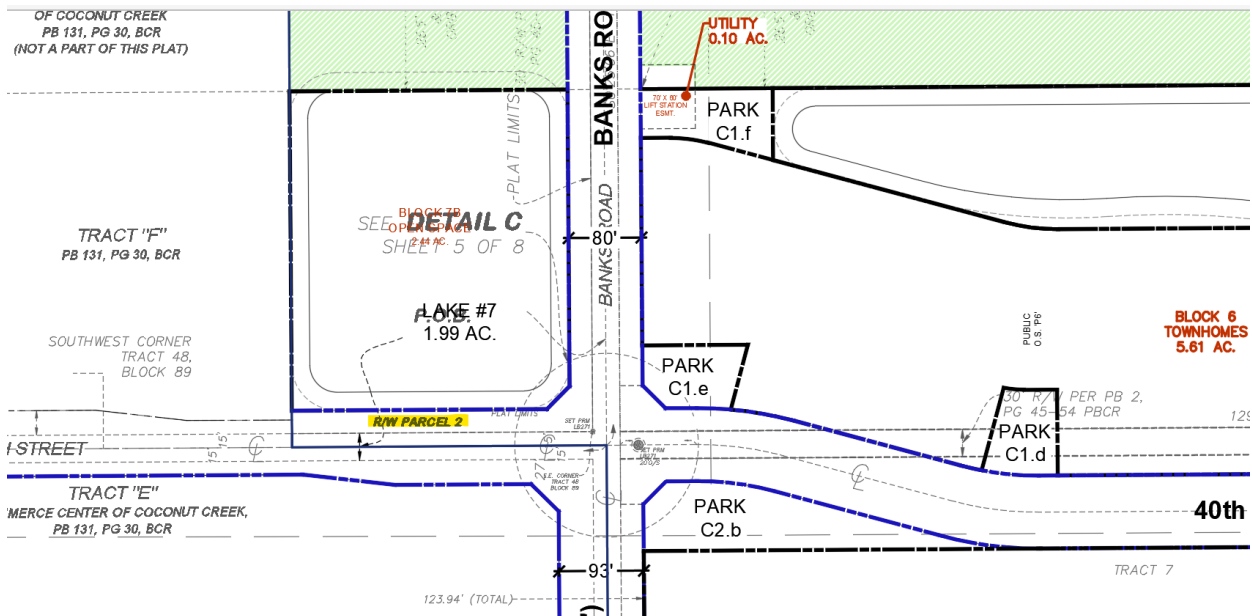
6. Does the Cullum Road segment of the approved plat, the legal description and proposed improvements harmonize with the roadways plan?

RESPONSE: The legal description matches the proposed roadway R/W design.

7. Does the NW 40th Street segment west of Banks Road and this legal description and proposed improvements harmonize with the roadways plan and approved plat?

RESPONSE: See below overlay of the proposed plat. R/W Parcel 2 is dedicated on the plat for 40th Street adjacent to open space Block 7B.

Beth please double check this response - I'm not sure the plat shows the 20' of R/W abandonment proposed adjacent of Block 9 (south 20' of existing R/W is proposed to be abandoned and that land is made part of the Greenway G9.1. What does the legal show?



URBAN DESIGN AND DEVELOPMENT

1. Roadways plan subject to City Commission approval.
RESPONSE: Comment acknowledged. Please schedule this application for Commission approval.
2. Any pending modifications to plans because of outstanding DRC comments or continued efforts to review and refine plans must be resubmitted as "final plans" prior to the issuance of a building permit for any specific block or other site plan application.
RESPONSE: The final design plans will include any DRC comments and conditions to be included in the permitted plans.
3. Additional comments may be provided upon review of any revised plans.
RESPONSE: Comment acknowledged.
4. Pending reimbursement to the City for professional review services prior to permit issuance.
RESPONSE: Please send the invoices to Rick Stephano at GSR RE.
5. Pending recorded public access easements.
RESPONSE: The roadways are within the R/W. The plat will record the new R/W areas when recorded.
6. Pending plat recordation.
RESPONSE: The plat is in process of final approval at Broward County and will be recorded in the future.
7. Pending applicant PowerPoint presentation at public meetings.
RESPONSE: The presentation will be provided to the city prior to the meetings.
8. Pending receipt of 1 digital and 13 printed application packages as previously discussed.
RESPONSE: The hard copies will be provided when city staff schedules the board meetings.
9. Pending submittal of proposed Amenities/Imaging Package including street signage for review.
RESPONSE: The applicant has been working with world class Branding and Urban Design consultants to create the required Amenities package. We look forward to presenting the Amenities package to the City.

10. Pending a revised corner design that creates a plaza instead of connecting sidewalks at Block 9 and Block 11 that provides a generally consistent streetscape through the civic node of MainStreet.
RESPONSE: Park G9.1 greenway design has been revised to address this comment and will be updated on the Parks Plan resubmittal once comments have been received.
11. Pending parks plan review and utilities conflicts resolution within planter islands and other green spaces.
RESPONSE: The utility has been relocated out of the islands.
12. Pending sidewalk on west side of Block 15B along NW 54th Terrace.
RESPONSE: Please see response to Engineering Comment #49 above. The sidewalk connectivity between Cullum Road and Wiles Road is already provided within Block 15B next to the parking lot as well as sidewalks within Village Square connecting to the Lake 15A greenway trail which then connects to Wiles Road. NW 54th Terrace is approved under the Mainstreet Design Standards as a rear alley with no sidewalks within the typical cross section. Sidewalk connectivity is provided within the Public rights of way Cullum, Village Square and through the abutting blocks.
13. Pending receipt of individual landscape data tables that separate and reflect required landscaping including trees and palms along the right-of-way for Monarch High School and Block 14 (City property).
RESPONSE: Proposed trees and landscape material along the Monarch High school strip has been separated from the overall C1 Type Street type calculations. The area along Block 14 is Park G14 ~~also~~ and is a part of the Parks Plan permit.
14. See landscape comments. Per discussion with applicant, review and revise landscape islands around the Village Green to provide larger shade trees.
RESPONSE: Structural soil is being provided for the planting areas around the Village Green roadway to accommodate larger shade trees in the terminal islands and landscape strips. See sheets LP-36 and LP-37.
15. Review and revise location of utility line at the northeast corner of the Village Green that runs underneath three (3) parking islands that prevent the installation of trees. See comment above.
RESPONSE: The utility drainage pipe has been relocated out of the islands.

Thank you for your consideration.

Very truly yours,
HSQ GROUP, LLC



Jay Huebner, PE, AICP, LEED AP



August 9, 2024

CITY OF COCONUT CREEK
Department of Sustainable Development
Coconut Creek, FL 33063
Attn: Liz Aguiar

Project Name: MainStreet Roadways
Project Number: PZ-22120002
Location: Cullum Rd and Banks Road
Applicant/Agent: HSQ Group
Review/Application: PMDD Site Plan

Ms. Aguiar,

We have reviewed the Review #4 comments dated June 26, 2024, and our responses are as follows:

ENGINEERING

General Comments:

- (Tier 2)** Please note that all reviews and comments are preliminary and the plans are subject to further review for compliance with the City's Code of Ordinances, Utilities and Engineering Standards Manual, and Mainstreet Design Standards upon any resubmittal of the site plans and at the final engineering review.
RESPONSE: Final engineering plans meeting City codes and ordinances will be submitted during the construction permitting process.
- (Tier 3)** All required approvals from FDOT, FDEP, Broward County, COCOMAR or any other applicable agencies must be obtained and submitted to the Engineering Division prior to issuance of Engineering permits.
RESPONSE: Permits from the other governmental agencies will be obtained prior to issuance of permits.
- (Tier 3)** In accordance with the City's code sec. 13-186, the execution of Performance Bonds for all proposed public improvements are required prior to issuance of Engineering Permit.
RESPONSE: Bonds will be posted prior to construction commencement.
- (Tier 2)** Please provide reclaimed water and irrigation plans.
RESPONSE: Reclaim water truck lines are shown on the plans. The irrigation for the green areas within the roadway will be provided at time of permit.
- (Tier 3)** Per Section 13-273 of City Code, shop drawings for all proposed Water, Wastewater, Reclaimed water, and Drainage must be submitted prior to issuance of engineering permit.
RESPONSE: Shop drawings will be provided prior to permit issuance.

6. **(Tier 2)** Provide accurate station and offset numbers throughout all sheets.
RESPONSE: Plan and profile design plans with station and offset will be provided on final engineering plans.
7. **(Tier 2)** Please specify the number and size of the proposed conduit banks under the sidewalks, coordination with communication companies is required to ensure size, location, and number of conduits are adequate for their needs.
RESPONSE: We are coordinating with the utility companies to determine the right amount of conduits. As of now we assume 5 -2" conduits bank.
8. **(Tier 1)** All proposed conduit banks needs to be placed below all proposed City utilities.
RESPONSE: Where possible the conduit banks will be placed under the City utilities; however, locations where there is gravity sewer or drainage that is located deeper, the sewer and drainage facilities will be below the conduits.
9. **(Tier 1)** During the roadway construction, all proposed utilities within the roadways must be installed and extended with stubs up to each block limit, including City parcels (notations need to be added to the plans). The proposed utilities must connect to the adjacent parcels to ensure the system is looped.
RESPONSE: The plans have been revised where needed to extend the utilities servicing the adjacent blocks into the blocks instead of within the Right-of-way.
10. **(Tier 2)** Provide irrigation plans and booster pump station locations with filtration systems.
RESPONSE: Irrigation plans for the landscaping within the R/W will be provided during the building permit process.
11. **(Tier 2)** Ensure all proposed sidewalks comply with ADA standards, including sidewalks containing mast arms.
RESPONSE: The final design of the sidewalks and ramps in coordination with the final location of the mast arms will be provided at final engineering plans and permitting stage.
12. **(Tier 1)** Previous comment has not been addressed: Please ensure that all plans are consistent based on the latest revisions. Landscaping plans and some cross sections are inconsistent.
RESPONSE: The landscape plan cross sections and civil plan cross sections have been coordinated.
13. **(Tier 1)** Previous comment has not been addressed: Provided cross sections are not consistent with the site plans, dimensions of sidewalks on cross sections and utilities are different from the plans. Please provide a consistent submittal.
RESPONSE: The cross sections have been updated to match the plan view.
14. **(Tier 2)** City staff has numerous comments on the utilities, which are in conflict with the roadway plans. Additional review will be required based on proposed master utility plans.
RESPONSE: Final engineering plans will have conflict information to make sure the utilities meet required separation distances.

15. **(Tier 2)** Ensure that all proposed utility locations shown on the cross-sections match what is on the site plan locations.
RESPONSE: The final engineering plans will include all utility locations on plans and cross sections. The final engineering plans will be plan and profile layout for ease of construction.
16. **(Tier 2)** Landscape, lighting, and/or irrigation shall not conflict with and shall maintain 10 feet preferred (minimum 6 feet) of separation from existing and proposed water, wastewater, and drainage.
RESPONSE: The final engineering and landscape plans will meet this criteria.
17. **(Tier 3)** Please be aware that all Utilities & Engineering standard details have been revised, and engineering staff will provide a copy of the latest revisions. All standard details are subject to change until they are approved and codified.
RESPONSE: The new standards have been included in our final engineering plans.
18. **(Tier 1)** City staff recommends the existing roadways are fully milled and repaved for aesthetic purposes.
RESPONSE: Areas that are impacted by the project will be milled and repaved; however, areas outside of the area of impact will not be included within the project scope. The areas along Cullum Road extending to 441 and along 40th Street extending to 54th Avenue will be identified for milling and repaving, as work to be done by others. These areas will be included as part of the City's responsibility on the cost sharing analysis sheet.
19. **(Tier 2)** Please ensure that all parallel ADA parking spaces meet ADA standards for access aisle.
RESPONSE: The ADA parking meets ADA standards.
20. **(Tier 3)** Roadways abutting county/state R.O.Ws must obtain approval from corresponding agency.
RESPONSE: All government agency permits required will be obtained prior to construction commencement.
21. **(Tier 1)** Previous comment has not been addressed: For all cross sections, please specifically annotate **limerock** base for all LBR 100.
RESPONSE: The pavement specifications require LBR 100 for rock base.
22. **(Tier 1)** After further review from City Staff, proposed forcemains are required to be epoxy coated DIP. Please revise annotations and cross sections accordingly.
RESPONSE: The notes will be changed to DIP.
23. **(Tier 1)** Remove all annotations for HSQ Group LLC, alignment.
RESPONSE: ok will be used for station and offset on final design.
24. **(Tier 2)** Crosswalk symbol is not consistent with the crosswalk pattern throughout the City. Existing patterns are herringbone design versus a brick pattern.
RESPONSE: The pattern has been updated to herringbone and design with Paveway treatment system will meet the standard pattern for the city.

25. **(Tier 1)** Previous comment has not been addressed: Standard detail for midblock crossing still calls out stamped asphalt pavement not the Paveway Treatment System.
RESPONSE: The note has been changed to Paveway treatment system.
26. **(Tier 2)** Please provide the standard detail for the crosswalks showing the Paveway Treatment System.
RESPONSE: see PM-1 for details.
27. **(Tier 1)** All proposed parallel ADA parking landing areas need to be stripped accordingly.
RESPONSE: The handicap sign is located in the stall and access is stripped.
28. **(Tier 1)** Please remove the annotation from all sheets stating, "Alternative pipe materials may be used depending upon availability of construction materials:
• C-900 for DIP. ;
• C.A.P. or H.D.P.E for R.C.P"
RESPONSE: These notes will be removed from the plan. Per discussion with staff, alternatives may be allowed subject to City approval should the required material not be available at time of construction. As such, a general note will be added to the plans that alternate materials may be used with prior approval by the City should the required material not be available at time of construction.
29. **(Tier 2)** Clarify if the proposed bus shelters will be utilized by the County or local transit.
RESPONSE: The bus shelters on Wiles and Lyons are County transportation. The interior bus shelters are local city transit.
30. **(Tier 1)** The provided pavement marking and signage plan has incorrectly labeled block numbers. Please correct accordingly.
RESPONSE: The block numbers have been corrected.
31. **(Tier 2)** Please ensure a minimum of 4 feet of separation is met between stop bars and crosswalks.
RESPONSE: The dimensions are on the plans for a minimum of 4'.
32. **(Tier 1)** Ensure there are no easements dedicated to utilities (drainage) within the City ROW. Landscape must also reflect these changes to guarantee consistency.
RESPONSE: No easements are shown in the R/W.
33. **(Tier 2)** All proposed water taps must be double valved, proposed plans call for single valves.
RESPONSE: Double valves will be included in the final engineering plans.
34. **(Tier 2)** Please provide pavement restoration details per the City of Coconut Creek's Standard Details.
RESPONSE: The detail will be added to the final engineering plans.
35. **(Tier 2)** Provide details for all proposed guardrails and bollards (removable and permanent).
RESPONSE: Please see sheet RD-4.

36. **(Tier 2)** For all proposed pavers, distinguish the hatching between permeable and non-permeable pavers.
RESPONSE: All pavers are permeable.
37. **(Tier 2)** Please provide additional elevations by the Final Engineering Review stage for all proposed roadways and drainage systems.
RESPONSE: Final engineering plans will have detailed grading included on them.
38. **(Tier 1)** Clarify different hatching symbols that are not shown on the legend for drainage pipes, for example along Cullum Rd.
RESPONSE: The large diameter pipes are shown to scale to ensure proper spacing to other utilities. They have a hatch within the pipe area.
39. **(Tier 1)** For all proposed 10x10 utility equipment easements, please ensure that there aren't any trees encroaching the proposed easement.
RESPONSE: The 10 x 10 utility equipment easement locations are placeholders until further coordination with FPL is done.
40. **(Tier 1)** Reclaimed water fire hydrants are not shown anywhere within the layout. They must be shown with the underground utility plan.
RESPONSE: Per discussion with staff, reclaimed water fire hydrants will be added every 1000 feet to allow for flushing. The detail will show that it is a reclaimed water hydrant (purple).
41. **(Tier 1)** It is recommended to have the reclaimed water main stub "off-set" to the back of F-curb, or installed parallel to the forcemain (per F.A.C. statutes) for future connection. By extending the (reclaimed water main) RWM to SR-7, this will yield any future impacts to the roadway.
RESPONSE: The RWL extends to the West end of the proposed development. We moved the end of the main into the grass area behind the curb to avoid future roadway impacts and added a re-use hydrant.
42. **(Tier 1)** For all proposed reclaimed water main dead ends, propose a corresponding blow off assembly or automatic flusher.
RESPONSE: A re-use hydrant has been added to the ends of the reclaimed lines for blow off purposes.
43. **(Tier 1)** Advance signage must be installed for all midblock crosswalks.
RESPONSE: see attached Pavement marking plans for locations.
44. **(Tier 2)** Drainage structures must not be in conflict with the proposed pavement markings.
RESPONSE: Comment acknowledged.
45. **(Tier 1)** Identify the bold black lines located within the roadways.
RESPONSE: Pavement markings.

City Market Ave:

46. **(Tier 1)** Sheet SP-10: Existing traffic separator on Sample Rd should be modified to prohibit illegal Left-turn onto east Sample Rd. A "reduce conflict intersection median" should be considered.
RESPONSE: As discussed, a raised median is proposed at the south end of City Market at Sample Road to prevent an illegal left-turn. The lane is being shifted and the median expanded per a request from FDOT, which is reflected on the revised plans.

47. **(Tier 1)** Sheet LP-17: Proposed tree is in conflict with the proposed 8" water main that is running west, please provide appropriate separations or provide root barriers as applicable.
RESPONSE: Trees have been shifted from water main.

NW 40th Street:

48. **(Tier 2)** Sheet SP-18: Propose a crosswalk for pedestrians crossing NW 40th St (North to South).
RESPONSE: The crosswalk has been added to the plans.
49. **(Tier 2)** Sheet SP-19: Further review will be required for the note about the proposed ramps and crosswalks by others.
RESPONSE: The ramp on the East side will be installed by the developer. The sidewalk adjacent to the City parcel is not part of this plan, but per discussions with the City it is shown for coordination purposes since it is outside of the project limits. This can be included as a city cost item if the city wants it constructed with the overall site.
50. **(Tier 2)** Please ensure that all proposed trees in the medians must maintain appropriate separations from proposed City utilities.
RESPONSE: The trees meet the minimum separation requirements.
51. **(Tier 2)** CS A4-A4: N.W.40th St west of Lyons Rd- Monument sign within the median does not show any footer or applicable conduits, lighting etc. In addition, irrigation piping are not shown.
RESPONSE: This is a place holder for now and the final signage design will be submitted separately for permit.
52. **(Tier 1)** The crosswalk added between Blocks 2 and 3 show colored pavers. Please clarify the actual color to be utilized.
RESPONSE: The color has been removed to avoid confusion. It will be the same color as the other on site cross walks.
53. **(Tier 1)** Sheet LP-4: Remove the 20' storm water utility easement on City Market Ave.
RESPONSE: The utility easement has been removed from the street.

Banks Road:

54. **(Tier 1)** Sheet SP-11: Previous comment has not been addressed, the separation must be a minimum of 6 feet, not 5 feet as proposed: Proposed fire hydrant is in conflict with existing stormwater line. Please revise location and apply appropriate separations.
RESPONSE: The fire hydrant has been moved to 6' away from the drainage pipe .
55. **(Tier 1)** Sheet SP-10: Please cite the design standards for the location of the bike lane located on the outside of the right turn lane. Bike lanes are typically located on the outside of the lane.
RESPONSE: The applicant acknowledges that the bike lane is typically on the outside of the lane when there is an adjacent through lane; however, traffic in this location is forced to make a right turn. As such, keeping the bike lane closer to the curb line creates the safest condition for the cyclist, and allows them to seamlessly transition into the existing bike lane on Sample Road. The bike lane has been shifted to the west per a request from FDOT to shift the turn lane, which is reflected on the updated plan set.

56. **(Tier 1)** Sheet SP-13: Proposed lift-station does not show any receiving manholes adjacent to the lift station.
RESPONSE: The receiving manhole is located in the Street. A note has been added to the plans.
57. **(Tier 1)** The proposed wastewater line segment from the receiving manhole leading into the lift station must be C-900 (green).
RESPONSE: A note has been added to the plan.
58. **(Tier 1)** Sheet SP-13: Include an item in the legend to identify the material used for the pattern shown for the FPL Maintenance Road.
RESPONSE: The legend has been updated to include the FPL maintenance road pattern.
59. **(Tier 1)** Sheet SP-17: Reclaimed water stub must be extended up to Wiles Rd. in order to prevent impact to the pavement and crosswalk.
RESPONSE: The reclaim water main has been extended North of the crosswalk.

Cullum Road:

60. **(Tier 1)** Sheets SP-1 and SP-2: Add the annotation and hatching layer for the milling and resurfacing of the entire lane on Cullum Road.
RESPONSE: The hatching for the milling and resurfacing has been added for this section and notes that this section is by the city to be included in the cost sharing analysis.
61. **(Tier 2)** City staff suggests to mill and resurface all lanes starting from Banks Rd to State Rd 7.
RESPONSE: The referenced areas along Cullum Road have been identified for milling and repaving, as work to be done by others. As these areas are outside of the project area, they will be included as part of the City's responsibility on the cost sharing analysis sheet.
62. **(Tier 1)** The existing reclaimed water main line along the north side of Cullum Road is to be included within the cross sections.
RESPONSE: The reclaimed water main has been added to the cross section.
63. **(Tier 1)** Proposed traffic calming devices must be coordinated with the proposed driveway for the new fire station on Block 14.
RESPONSE: City staff to provide CAD of site plan for Block 14 to add driveway opening to plan.
64. **(Tier 1)** Traffic control median should be mountable (A or B) curb as oppose to D-curb.
RESPONSE: Per discussion with City staff, the D-curb will remain to prevent cars from driving onto the median and hitting the trees.
65. **(Tier 2)** Clarification is required for all of the F-curb inlets catch-basins connectivity along the North side of Cullum Rd. Proposed drainage structures and the existing drainage manholes will be in conflict due to the their spread footer design.
RESPONSE: Final design plans will resolve this issue.

NW 54th Terrace:

66. **(Tier 1)** Sheet SP-32: The proposed sidewalk south of NW 54th Terrace needs to be continuous to the greenway adjacent to Lake 15A.

RESPONSE: The area adjacent to parcel 15B is severely constrained and it is not feasible to provide a sidewalk in the referenced location without having a significant impact on the ability to landscape this area. In addition, the type of traffic along the NW 54th Terrace alley is likely to be larger trucks accessing the lift station and temporary fire station. As such, locating pedestrian traffic away from the vehicular traffic provides a safer and more pleasant pedestrian experience.

There are two alternative pedestrian paths to connect from the proposed sidewalk on the east side of the southern portion of NW 54th Terrace adjacent to the parking garage to Wiles Road. The 1st is a path through the clubhouse parcel, and the second is a path through the plaza north of the Village Green, both of which lead to the lake greenway. Please see the connectivity diagram showing the pedestrian connectivity in this area. The applicant is committed to providing wayfinding signage that directs pedestrians to utilize these paths to access the greenway, which will create much more pleasant pedestrian experiences than a sidewalk along the alley that would lead north toward the lift station.

67. **(Tier 2)** Sheet SP-32: Crosswalk at Village Square is not consistent with paver crosswalks site wide.

RESPONSE: Please see revised plan with cross walk.

68. **(Tier 1)** Sheet SP-32: Please relocate the proposed water valves encroaching the paver crosswalk and ADA ramp to minimize impacts.

RESPONSE: The valves have been moved out of the cross walk.

69. **(Tier 1)** Sheet SP-32: Depicts cross section D1 but cross section D1 is labeled as NW 48th Ave on sheet RD-3. Provide a cross section for NW 54th Terrace.

RESPONSE: The cross section has been corrected.

70. **(Tier 1)** Sheet SP-32: Comment has not been addressed: Drainage layout and profile does not match the cross section. The proposed drainage line and associated catch basins appear to be in the middle of the crowned road, whereas the road design cross section shows curb and gutter drainage.

RESPONSE: The cross section has been revised to match the plans.

71. **(Tier 2)** Sheet SP-33: Comment has not been addressed: One of the two existing City's forcemains is still not shown, please refer to City's GIS system to locate all existing missing City utilities.

RESPONSE: The 2nd force main will be added to the final engineering plans.

72. **(Tier 1)** Sheet LP-34: Comment has not been addressed: A tree is still proposed within the 15' utility easement on the west side of sheet, please relocate.

RESPONSE: The tree has been shifted from the utility easement.

NW 54th Ave:

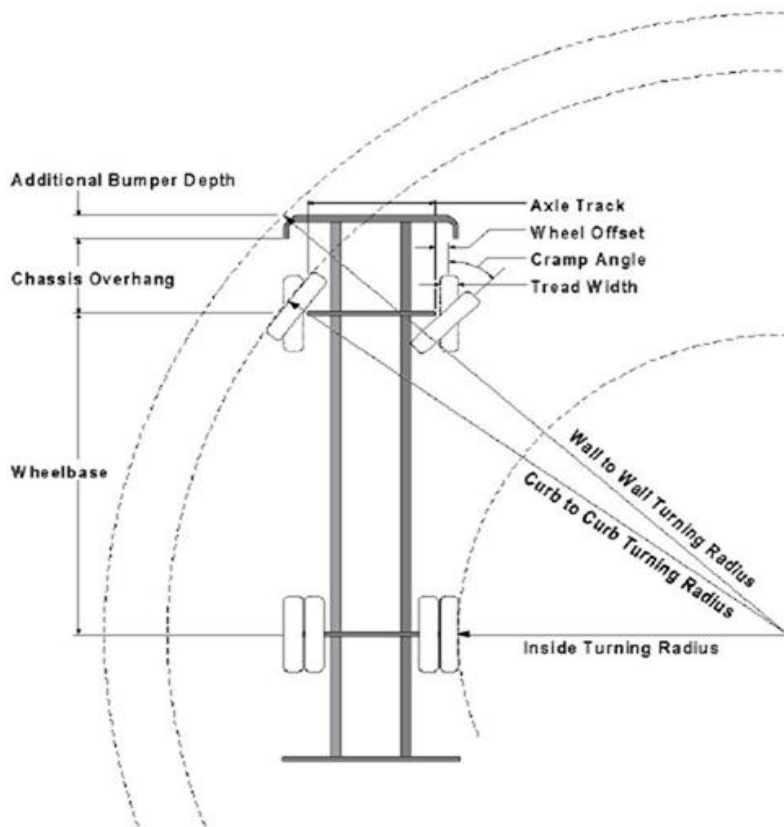
73. **(Tier 1)** Sheet SP-34: Please re-evaluate the left turn into the Seminole Casino and to possibly include a stop sign.

RESPONSE: As discussed with staff, the left turn movement is permissive and traffic will stop if there is oncoming traffic. As such, no stop sign is necessary.

74. **(Tier 1)** Sheet SP-34: Provide a solid line for the left and right turn only lanes, not skip lines as proposed.
RESPONSE: The solid white line has been added to the plans.
75. **(Tier 1)** Sheet SP-34: Bike lane transition is missing from the roadway to the multiuse path.
RESPONSE: The existing street South of our property does not have on street bike lanes at this time. Further discussion with City staff is needed to determine how to end the bike lane at our southern property line during final engineering plan design.

FIRE

1. For the traffic calming on Cullum Road., provide a Fire Truck Route Plan that can accommodate a two axle fire truck (B40 Bus template) that is 39 feet long, 9 feet wide. Display turning radius dimensions, front wheel path, rear wheel path, apparatus path, and front chassis overhang (see figure). Use a 50 foot outside 25 foot inside turning radius. Do not overlap the entrance and exit of the truck and provide multiple pages if necessary. (NFPA 1-18.2.3.1.1)
- Use AutoCAD or similar program to generate the fire truck route plan. Please include a depiction of the truck on the plan.



RESPONSE: Please see master roadway truck routing plan with the new medians and truck routing around them.

LANDSCAPING

1. General Note: Tree Disposition plans and mitigation will be required to be approved prior to site plan approval for the individual blocks, roadways, and greenway/ parks submittals. Comment remains pending approval of tree disposition and mitigation plans.
RESPONSE: Acknowledged.
2. Include Tree Soil Volume note from Block 8, sheet LP-9 on Roadway plans regarding minimum soil volumes per tree categories and the use of structural soil where required.
RESPONSE: Soil volume note has been added to sheet LP-38.
3. Clarify proposed paveway treatment system labeled on sheet SP-3 for crosswalks.
RESPONSE: Please see site plan for standard detail for Paveway system.
4. Comment not addressed. Provide paver edge restraint call out/ label on site plans and hardscape plans for proposed pavers adjacent to landscape areas.
RESPONSE: per the engineering department request, the multi-use sidewalk and bike lane will be concrete. The concrete sidewalk will act as an edge restraint adjacent to the permeable paver and landscape areas. The perimeter edge of the permeable pavers will be mudset in the areas adjacent to the landscape areas.
5. Comment not addressed. Landscape sheet numbers/ layouts are to match the Site Plan sheet numbers/ layouts.
RESPONSE: Refer to sheet 180-mainstreet landscape index for our cover sheet showing the key map and sheet numbers.
6. Proposed landscape within the ROW of Banks Road, City Market Avenue, and Cullum Road, which are bubbled and labeled as "Landscape Provided by Adjacent Property Owner", requires further discussion with the City and Applicant regarding responsibility.
RESPONSE: The applicant has illustrated the landscaping that is adjacent to property that is not owned by the applicant and is outside of the project area. These streetscapes are not the responsibility of the developer.
7. Streetscape tree requirements along Cullum are not being met as established for Street Type C-1 and C-3 within the tables provided on sheet LP-39. Tree quantities to be provided either within the ROW or an alternate location. Additional discussion required with the City and Applicant regarding planting requirements on Cullum Road.
RESPONSE: The plant requirements for these areas will be quantified and a note added requiring that the required landscape be planted somewhere else within the project. The first options will be locations that are close to Cullum; however, given that the applicant does not own the property north of Cullum, the landscaping may need to be located in another area.

The street type C-1 has numerous conditions that limit the available space for tree placement. On the north side of Cullum road, we have maximized the landscape area for trees along the monarch high school. To the west of Monarch high school, the area that could have been used for proposed trees is meeting the green space park plan requirements. We are using right tree right place principles and adding hibiscus standards and Indian hawthorn standards to provide visual screening and interest for the 5' landscape strip locations that we have available. On the South side of Cullum road, we have adjusted the sidewalk adjacent to the canal to allow for additional trees to be planted. In addition, medians have been added to slow traffic circulation and has cut into the open space available to plant larger trees. We have provided hibiscus and Indian hawthorn standards along the south side of the road to accommodate screening and right tree right place practices where the landscape strips do not meet minimum width to sustain a healthy root ball.

8. Sheet LP-39, table 2 under Street Type C4 Landscape Strip requirements, remove south reference and provide plant quantities for required streetscape adjacent to Block 12A.
RESPONSE: The landscape strip requirements for the streetscape adjacent to block 12a is being calculated under table 2 “adjacent to greenways” section on sheet LP-39.
9. Additional planting around the lift station, street trees, shrubs to be provided on sheet LP-35. Provide limits of adjacent block site plans. Comment remains, additional discussion required with City and Applicant to further define the area.
RESPONSE: We have provided additional screening adjacent to the lift station. Per the engineering department, we are unable to plant trees and palms around the lift station. See sheet LP-35.
10. Label and dimension all easements on sheet LP-35. Comment partially addressed. Multiple dashed lines are provided that are not identified.
RESPONSE: labels and dimensions have been added. See sheet LP-35.
11. On sheet LP-39, revise Miscellaneous note and General note stating planting soil depth to be a minimum of 18” for shrubs, hedges, and groundcovers. Revise shrub planting detail on sheet LP-38 accordingly. Comment partially addressed. 3rd note under General Notes on sheet LP-38 still state a 4” depth of soil.
RESPONSE: Note has been revised.
12. Comment not addressed. Revise native requirement table on sheet LP-3 to include all proposed shrubs, hedges, and groundcovers. Proposed native quantities do not meet the minimum 50%. In addition, remove “Excl. Groundcover” text from table.
RESPONSE: Native requirement table has been update to include all landscape shrub material in the calculations and exceeds the 50% native requirement. See sheet LP-38.
13. Provide note under tables regarding FPL easement restrictions (Max. 14’ trees at maturity and Right Tree-Right Place) that limit the use of required shade trees. Comment not addressed. Note not provided as stated in response letter.
RESPONSE: Notes have been provided on sheet LP-39 under the streets NW 48th Avenue and Banks Road, where FPL right tree, right place conditions are in place.
14. Proposed parking islands under Banks Road Street Type A, NW 48th Avenue Street Type B, and Village Square Street Type B do not meet the minimum parking island area proposed in the PMDD. Street Type A requires a minimum of 250 SF and Street Type B requires a minimum 140 SF. Comment not addressed as stated in response letter.
RESPONSE: Parking island calculations have been updated. See sheet LP-39.
15. Fill in missing numbers for “X” under NW 48th Avenue, Street Type B2, table 1. Comment not addressed as stated in response letter.
RESPONSE: Note has been updated. See sheet LP-39.
16. Per the PMDD, the allowed increase in proposed canopy coverage is based on the placement of trees within larger open areas that will allow for the larger mature canopy. The smaller canopy coverage square footage is to be used for trees placed within smaller landscape areas, parking lot islands, and or medians. Correct provided square footages under Green Plan Goal table. See Sustainability for additional

comments. Comment not addressed. Separate calculations have not been provided as stated in response letter.

RESPONSE: An open space exhibit has been provided showing the areas where category 1 and 2 trees are being calculated at the open space square footage. Category 1 and 2 trees that are being planted in terminal islands and medians are being calculated at the smaller square footage. See attached Open Space Exhibit.

17. Proposed tree quantity along NW 54th Terrace on sheets LP-34 and LP-35 are not consistent with total stated in landscape requirement table on sheet LP-39 and are missing required trees.

RESPONSE: The table has been updated. See sheet LP-39.

18. Sheet LP-21, confirm proposed light pole locations within FPL area have adequate separation from storm lines.

RESPONSE: Acknowledged.

19. Sheet LP-29, designate shifted location of required island shade tree for southeast bulb out of intersection due to underground canal pipe. Comment partially addressed. Designate/ label shifted tree on plans. Shifted tree may not count towards landscape strip requirements.

RESPONSE: (1) live oak that is being planted in the adjacent landscape strip is being calculated towards this terminal island tree. A note has been provided on the Cullum Road C3 (1) table on sheet LP-39.

20. Sheet LP-36, provide additional information on depth of proposed storm line. It appears the proposed trees with root barrier within the landscape islands do not have adequate planting space.

RESPONSE: The proposed trees are 6' from the storm line and root barriers have been provided.

21. On Sheet LP-36 and LP-37, for street type B-3, parking islands are required to provide 1 shade tree per island. Comment partially addressed. Applicant to submit a waiver request and justification for the tree category deviation.

RESPONSE: Acknowledged.

22. Proposed landscape adjacent to Al Hendrickson Toyota site on sheets LP-8 and LP-9 requires further coordination with the City and dealership applicant.

RESPONSE: This area is in for approval by the Al Hendrickson Toyota owner, and landscaping adjacent to their property will be addressed in their approvals. The notes on the landscape plans in this area will be adjusted to reflect that it is shown for illustrative purposes only.

23. Additional comments may be provided upon review of the site plan re-submittal.

RESPONSE: Acknowledged.

PHOTOMETRICS

1. The photometric plans for NW 48th Ave indicate several spots adjacent to the wetland area where the footcandles at the edge of the roadway are as high as 2.1fc. **Provide light shields for the fixtures adjacent to the wetland to accomplish the requirement of zero fc at the edge of the ROW.**

RESPONSE: The lights on the West side of NW 48th Avenue now include a shield to limit light towards the wetland buffer area.

SUSTAINABILITY

1. Sheet SPM-1 identifies 329 parallel parking stalls and 5 EV chargers. Using the agreed requirement of 10% of parking to be EV ready, the number of chargers should be 33. Indicate the number of EV charging stations planned for the roadway.

RESPONSE: As discussed with staff, the 10% applies to required parking within the blocks, and is not applicable to the parallel parking within the rights-of-way. The PMDD requires that EV parking be provided at strategic locations throughout the project area, which is accomplished with the provision of EV spaces at the Main Plaza, Johns Park, and pocket parks along 40th Street. Each EV charging station provides for 2 cars to charge at each location, thus providing 10 EV spaces. In addition, the Block 2 site plan will be updated to provide additional EV parking within the surplus parking area adjacent to the FPL linear park, subject to FPL approval. Lastly, an additional EV charger is now provided near the Village Green. An additional EV charger has been added on Cullum Road adjacent to the Block 11 park. This is an additional 4 EV charging spaces on the North end of the site open to the public.

5. Alternative Transportation

- Protected bike paths and walkable greenways aimed at promoting alternative transportation shall be provided community wide and serve as a primary focus of the development.
 - Bicycle racks shall be located at strategic locations throughout the development's open spaces to provide the necessary parking for cyclists. Conceptual locations of the bike racks are identified as element S8 on the Master Conceptual Sustainability Plan.
 - Electric vehicle ("EV") charging stations shall be located at strategic locations throughout the development to provide parking for alternative vehicles.
 - A minimum of two (2) EV charging stations with four (4) plugs shall be provided prior to the issuance of the first Certificate of Occupancy for each commercial, multifamily residential, and civic block.
 - At least one (1) ADA accessible EV charging stall shall be provided in each commercial, multifamily residential, and civic block.
 - A minimum of ten percent (10%) of the required parking spaces for each commercial, multifamily residential, and civic block shall be EV ready. EV Ready for commercial, multifamily residential, and civic uses is defined as providing underground conduit to the parking stall to provide for future addition of EV facilities. All site plans shall identify the location of EV Ready charging locations with a note that commits to the conduit required.
 - All townhome and villa units shall be EV Ready. EV Ready for townhome and villa units shall be defined as provision of a sufficient power supply for addition of an EV charger by an individual unit owner.
 - Ride share pick-up/drop-off locations shall be provided at strategic locations throughout the development to support alternative transportation. These pick-up/drop-off locations may be provided in conjunction with City shuttle stops.
 - Conceptual locations of the charging stations are identified as element S9 on the Master Conceptual Sustainability Plan.
2. Action 2.1 – Achieve 40% tree canopy coverage throughout the City: This calculation must use Broward County's canopy square footage. The higher square footage allowance is meant for trees in large open spaces, such as parks. The larger allowance does not apply to the bulk of the roadways. See landscape notes.
RESPONSE: The PMDD establishes the canopy square footage to be used for this calculation. The plans will be modified to clarify which trees are being calculated using the larger square footage.

TRANSPORTATION

1. Provide an update to the Broward County Plat Review comments and/or status of the County's plat review;
RESPONSE: The plat has been reviewed by Broward County and is in process to be placed on the County Commission board meeting as of the date of this letter.
2. Provide an update or a narrative summary to the FDOT meeting for the City Market Avenue's connection Sample Road and WB Sample Road right-turn lane;
RESPONSE: FDOT has issued a letter accepting the right-in, right -out on City Market Avenue and Sample Road and right-in/ Right-out left-in on Sample and Banks. See attached letter from the FDOT.
3. Can the design team allow the Roadway plans to include other significant improvements such as the roundabout at NW 54th Avenue/NW 40th Street along with the section of NW 40th Street from Banks Road to NW 54th Avenue? Can these additional roadway improvement be included with the plans with a note "to be constructed by others"?
RESPONSE: As discussed, this area is outside of the project area and the work is not included within the project scope. As such, to avoid delays in the review/approval process and ensure no confusion as to responsibility to construct, the applicant is not including these improvement on the plans for this site plan approval.
4. The traffic calming features included along Cullum Road shall identify the future driveway into the City parcel; and
RESPONSE: The future driveway to the City parcel shall be provided to us in CAD format to incorporate into the design and modify any Cullum road features as needed.
5. Additional comments may be forthcoming.
RESPONSE: Comment acknowledged.

URBAN DESIGN AND DEVELOPMENT

Pending

1. Roadways plan subject to City Commission approval.
RESPONSE: Comment acknowledged.
2. Additional comments may be provided upon review of any revised plans.
RESPONSE: Comment acknowledged.
3. Reimbursement to the City for professional review services prior to permit issuance.
RESPONSE: Comment acknowledged.
4. Recorded public access easements.
RESPONSE: The roadways are public and do not require access easements.

5. Plat recordation.
RESPONSE: The plat is in process for county approval and recordation.
6. Applicant PowerPoint presentation at public meetings.
RESPONSE: A PowerPoint presentation will be provided prior to the board meetings.
7. Receipt of 1 digital and 13 printed application packages as previously discussed.
RESPONSE: The plans will be submitted when City staff schedules the board meetings .
8. Submittal of proposed Amenities/Imaging Package including street signage for review.
RESPONSE: The amenities package is coordinated and submitted separately to the city for review.
9. Parks plan review and utilities conflicts within green spaces.
RESPONSE: A separate parks site plan application is under review by the City and will address this comment.

Thank you for your consideration.

Very truly yours,
HSQ GROUP, LLC

A handwritten signature in black ink that reads "Jay Huebner". The signature is written in a cursive, flowing style.

Jay Huebner, PE, A.I.C.P., LEED AP



May 15, 2024

CITY OF COCONUT CREEK
Department of Sustainable Development
4800 West Copans Road
Coconut Creek, FL 33063
Attn: Liz Aguiar

Project Name: MainStreet Roadways
Project Number: PZ-22120002
Location: Cullum Rd and Banks Road
Review/Application: PMDD Site Plan

Ms. Aguiar,

We have reviewed the DRC Review #3 comments dated February 27, 2024, and our responses are as follows:

ENGINEERING

General Comments

- (Tier 2)** Please note that all reviews and comments are preliminary and the plans are subject to further review for compliance with the City's Code of Ordinances, Utilities and Engineering Standards Manual, and Mainstreet Design Standards upon any resubmittal of the site plans and at the final engineering review.
RESPONSE: Final design plans will be provided at the permitting phase of this project.
- (Tier 1-2)** Please note that as the site plan review was completed prior to the plat review and approval, all engineering comments are preliminary and may be modified upon further reviews as well as more comments may be generated.
RESPONSE: Final design plans will be provided at the permitting phase of this project.
- (Tier 3)** All required approvals from FDOT, FDEP, Broward County, COCOMAR or any other applicable agencies must be obtained and submitted to the Engineering Division prior to issuance of Engineering permits.
RESPONSE: Permits from other agencies will be provided to the city during the permitting phase.
- (Tier 3)** In accordance with the City's code sec. 13-186, the execution of Performance Bonds for all proposed public improvements are required prior to issuance of Engineering Permit.
RESPONSE: the bonds will be posted prior to construction commencement.
- (Tier 2)** Please provide reclaimed water and irrigation plans.
RESPONSE: Plans will be provided at the permitting phase.

6. **(Tier 3)** Per Section 13-273 of City Code, shop drawings for all proposed Water, Wastewater, Reclaimed water, and Drainage must be submitted prior to issuance of engineering permit.
RESPONSE: shop drawings will be submitted during the engineering permit process.
7. **(Tier 2)** Provide accurate station and offset numbers throughout all sheets.
RESPONSE: final engineering plans will include this information.
8. **(Tier 2)** Please specify the number and size of the proposed conduit banks under the sidewalks, coordination with communication companies is required to ensure size, location, and number of conduits are adequate for their needs.
RESPONSE: We are coordinating with the utility companies on the number of conduits needed to service the property.
9. **(Tier 2)** Provide irrigation plans and booster pump station locations with filtration systems.
RESPONSE: These plans will be provided during the permitting process.
10. **(Tier 2)** Ensure all proposed sidewalks comply with ADA standards, including sidewalks containing mast arms.
RESPONSE: the sidewalks will meet the ADA standards.
11. **(Tier 1)** Please ensure that all plans are consistent based on the latest revisions. Landscaping plans and some cross sections are inconsistent.
RESPONSE: Plans have been coordinated between civil and landscaping.
12. **(Tier 1)** Provided cross sections are not consistent with the site plans, dimensions of sidewalks on cross sections and utilities are different from the plans. Please provide a consistent submittal.
RESPONSE: The cross sections have been updated with the latest plan design.
13. **(Tier 2)** City staff has numerous comments on the utilities, which are in conflict with the roadway plans. Additional review may be required based on proposed master utility plans.
RESPONSE: Final design and coordination with utilities and engineering will be done during the permitting process.
14. **(Tier 2)** Ensure that all proposed utility locations shown on the cross-sections match what is on the site plan locations.
RESPONSE: The cross sections have been updated to match the plan view including the utility locations.
15. **(Tier 2)** Landscape, lighting, and/or irrigation shall not conflict with and shall maintain 10 feet preferred (minimum 6 feet) of separation from existing and proposed water, wastewater, and drainage.
RESPONSE: The minimum separations have been maintained on the plans.
16. **(Tier 3)** Please be aware that all Utilities & Engineering standard details have been revised, and engineering staff will provide a copy of the latest revisions. All standard details are subject to change until they are approved and codified.
RESPONSE: HSQ has requested the latest details for the final engineering plans.

17. **(Tier 1)** Plans still call-out stamped asphalt pavement, however, the City does not utilize this methodology anymore due to color failure/deterioration (example is located on Coconut Creek Parkway).
RESPONSE: The new method which is being utilized is the Paveway Treatment System.
18. **(Tier 1)** Please reference location of note added: Please ensure that all existing roadways that are expected to be impacted are fully milled and repaved per City Standards.
RESPONSE: the notes have been added to Cullum Road where the force main is extended. The milling and resurfacing is only for the area impacted by construction for the width of the travel lane, not the entire roadway.
19. **(Tier 1)** After discussion with transportation reviewer, additional traffic calming on Cullum Road is required.
RESPONSE: Per our coordination with City staff the new design to include meandering road and landscape islands meets the intent to slow traffic on Cullum Road between Banks and Lyons Roads.
20. **(Tier 2)** For all proposed curbing, please ensure details are reflected on the cross sections as well, specifically the limerock pad.
RESPONSE: The curbing details now have the 4" rock pad under the curb.
21. **(Tier 2)** Please ensure that all parallel ADA parking spaces meet ADA standards for access aisle.
RESPONSE: Please see details for flush transition between the ADA parking stall and the sidewalk meeting ADA standards.
22. **(Tier 3)** Roadways abutting county/state R.O.Ws must obtain approval from corresponding agency.
RESPONSE: Permits from the other agencies will be obtained and a copy provided to the city during the permitting process.
23. **(Tier 1)** Please explain the purpose of the proposed concrete sawcut line within the multiuse pathway.
RESPONSE: The path consists of bike lane and pedestrian lane. The sawcut is strategically placed to represent the dividing line between the two uses. We thought about painting the line, but other paths like this one utilizing paint tends to fade over time and looks bad and creates more maintenance costs. The sawcut is needed anyway for the concrete design so placing it at that location serves both purposes.
24. **(Tier 1)** Please show transitions from red Venetian sidewalk to regular concrete sidewalk.
RESPONSE: Please see the plans in color with red sidewalk pattern for the venetian sidewalk. The remainder of the white sidewalks do not have this color on the plans.
25. **(Tier 2)** All D-curbs must be monolithically poured (same mix design) due to the permeable pavers installation. If this is scope of work is NOT accomplished, the results will yield a cold joint between the upper 6" reveal and the bottom 12" footer. When this scenario occurs the surface water will leach out onto the paved area creating an unsightly appearance of algae & a slip hazard; which will require constant maintenance by City staff.
RESPONSE: The D curb detail is monolithic poured design.

26. **(Tier 1)** Adjustment of the lime-rock curb-pad must be completed within the cross-sections. It appears to show that the lime-rock pad is within the base-rock elevation and this is not accurate. The lime-rock pad must extend into the sub-grade material to be in compliance with industry standards.
RESPONSE: Please see revised sections with the rock extension into the subgrade.
27. **(Tier 1)** Alternative disclaimer NOTE **must** be removed due to the possibility of material shortage and the use of other/substitute product. This changes the burial depth requirements and load calculation for the PVC, C.A.P., HDPE, DIP etc. All alternative materials must be reviewed and approved by the City Engineer.
RESPONSE: The note has been removed.
28. **(Tier 1)** All D-curbs depicts a lime-rock pad at the base of the footer. Please clarify the justification for such installation.
RESPONSE: The rock pad has been extended below the curb.
29. **(Tier 2)** Compacted sub-grade annotation for the sidewalk is a bit conflicting based on the material description (LBR-40). Compacted sub-grade follows ASSTHO T-99 standards and NOT a T-180 requirements. Clarification is required within the annotations.
RESPONSE: The note has been changed to Stabilized subgrade.
30. **(Tier 1)** For all cross sections, please specifically annotate limerock base for all LBR 100.
RESPONSE: All cross sections have been revised.
31. **(Tier 1)** All proposed water main trunk lines on the master roadway plans must be 12" minimum. Cross sections must also show consistency with 12" water mains.
RESPONSE: Per our discussions with staff the water mains have been changed to 8" diameter.
32. **(Tier 1)** Per AWWA and FDEP requirements, burial depth details for PVC pipe are necessary; sand, tracer wire, marking tape, or pull boxes and hand holes.
RESPONSE: The PVC pipe will be buried at the appropriate depth with tracer wire and marking tape.
33. **(Tier 1)** City staff has concerns on the safety of the transition from the shared use path to the bike lane on the roadway at un-signalized intersections since traffic signals may not be warranted.
RESPONSE: The design follows standard FDOT bike lane layout. The transition to the Lyons Road, Wiles Road and Sample Road is necessary to match the existing bike lane in the County roads.
34. **(Tier 1)** Provide sight triangles per FDOT standards.
RESPONSE: Per our conversation with City staff, we are using 40' sight triangles.
35. **(Tier 1)** Provide a notation for the minimum cover for the proposed conduit banks on all cross sections.
RESPONSE: the 30" cover dimension has been added to the cross sections.
36. **(Tier 1)** Plans depict C-900 forcemains on multiple cross sections and site plan sheets. The City does not utilize this material for such installation and only utilizes epoxy coated D.I.P. or HDPE.
RESPONSE: The force main material has been changed to HDPE.

37. **(Tier 1)** Please move all valves out of decorative/paver crosswalks.
RESPONSE: The valves have been moved out of the cross walks and pavers.
38. **(Tier 1)** Please clarify who is responsible for maintenance obligation of the beacon lights along the crosswalks. Please clarify if the beacon lights are solar operated.
RESPONSE: The city is responsible for maintaining the beacon lights within City R/W. The lights are solar operated.

City Market Ave:

39. **(Tier 2)** On Sheet SP-28: Annotate a minimum of 4 ft. set back from all stop bars to crosswalks.
RESPONSE: The 4' setback has been added to the plan.
40. **(Tier 2)** CS CM1-CM1: Please note that the 12' utility easement shown on the cross section must be dedicated as right of way.
RESPONSE: This is not part of the PMDD Mainstreet project and is owned by the adjacent property owner. Please contact them to dedicate the property.
41. **(Tier 1)** Show continuity for the bike path, bike path discontinues at connection to Monarch Station sidewalk.
RESPONSE: Per our conversation with City staff the connection for the bike path is to the existing Monarch Station sidewalk. The city doesn't want to pay for additional bike lanes, sidewalks and on street parking at this time.
42. **(Tier 1)** On Sheet SP-28: Annotate whether the ADA mat is proposed or existing.
RESPONSE: The mat is proposed as noted.
43. **(Tier 1)** CS CM3-CM3: Provide adequate cover for the proposed 60" RCP drainage pipe.
RESPONSE: The drainage pipe has been shown with adequate cover and has a minimum of 3' over cover under the roadway.

NW 40th Street:

44. **(Tier 2)** Sheet SP-18: ADA ramp and cross walk are missing (North to South crossing NW 40th St and East to West crossing NW 54th Ave)
RESPONSE: The existing sidewalk detectible warning has been added to the plan.
45. **(Tier 1)** Sheet SP-19: Please show dimensions (length and width) of the proposed parallel parking, including all ADA spaces.
RESPONSE: The parking stall has been dimensioned.
46. **(Tier 1)** Sheet SP-19: Please clarify the note about proposed ramps and crosswalks by others.
RESPONSE: Per our conversation with City staff requesting improvements outside of the PMDD boundary, we show the improvements by others since it is not the developer's responsibility to construct it.

47. **(Tier 1)** Sheet SP-20: Proposed water easement is encroaching the multiuse path sidewalk, easements are not needed on R.O.Ws.
RESPONSE: The easement has been removed from the R/W.
48. **(Tier 1)** Sheet SP-21: Proposed 5 feet sidewalk and ramp along NW 40th Street (East of City Market Ave) leads to nowhere, please clarify.
RESPONSE: That is the bus stop location to the bus shelter.
49. **(Tier 2)** Please ensure that all proposed trees in the medians must maintain appropriate separations from proposed City utilities.
RESPONSE: The trees are designed to be 10 feet from the utilities.
50. **(Tier 2)** CS A4-A4: N.W.40th St west of Lyons Rd- Monument sign within the median does not show any footer or applicable conduits, lighting etc. In addition, irrigation piping are not shown.
RESPONSE: The section is for location only. The sign has not been designed yet.
51. **(Tier 1)** City staff has concerns on the safety of the lack of pedestrian crosswalks between Block 2 and 3.
RESPONSE: A crosswalk has been added between Blocks 2 and 3 per our discussions with City staff.
52. **(Tier 1)** Sheet LP-4: Public art is proposed right above existing storm water main, within easement. Please relocate as permanent structures are not permitted within easements.
RESPONSE: The public art has been moved out of the easement.

Banks Road:

53. **(Tier 1)** Sheet SP-11: Proposed fire hydrant is in conflict with existing stormwater line. Please revise location and apply appropriate separations.
RESPONSE: The fire hydrant has been moved away from the storm line.
54. **(Tier 1)** Sheet SP-11: Proposed water easement is within R.O.W, easements are not required within City R.O.Ws.
RESPONSE: The easement has been removed from the R.O.W.
55. **(Tier 2)** Sheet SP-12: Proposed light pole is placed on top of an existing drainage line, please relocate.
RESPONSE: The light pole has been moved away from the drainage line.
56. **(Tier 1)** Sheet SP-13: Proposed drainage easement is within City R.O.W, easements are not required within R.O.Ws.
RESPONSE: The easement has been removed from the R.O.W.
57. **(Tier 1)** Please provide a cross section between NW 40th and Cullum Road.
RESPONSE: Please see cross section B7-B7.
58. **(Tier 1)** Sheet SP-14: Proposed FPL transformers for Block 11 need to be located within an easement.
RESPONSE: The transformer for Block 11 has been moved to the interior of the Block.

59. **(Tier 1)** Sheet SP-10: Bike lane should be relocated to the outside of the turn lane or shared within the turn lane per MUTCD standards.
RESPONSE: The bike lane has been moved to the West side of the turn lane since it is a right turn only for bicyclists.
60. **(Tier 1)** CS B6-B6: Please callout the space between the 10' concrete sidewalk and the 11' drive.
RESPONSE: Please see revised cross section.
61. **(Tier 1)** CS B1-B1: Right turn arrow (11' drive isle) may be the incorrect symbol.
RESPONSE: Please see revised cross section.

Cullum Road:

62. **(Tier 1)** CS C3-C3: Proposed cable guardrail is not shown on the north side of the road.
RESPONSE: The guard rail has been added to the cross section.
63. **(Tier 1)** Please show all drainage easements west of NW 54th Ave.
RESPONSE: The drainage easements have been labeled.

NW 54th Terrace:

64. **(Tier 1)** Proposed parking stalls are not necessary for City staff.
RESPONSE: we are removing existing stalls for the existing fire station. These replace them until the fire station is relocated.
65. **(Tier 1)** Sheet SP-32: The proposed sidewalk south of NW 54th Terrace needs to be continuous to Wiles Road.
RESPONSE: The sidewalk continuity from Wiles to Cullum is along the lake front to NW 54th Avenue. NW 54th Terrace is an alley not considered for pedestrian traffic. We do not want to promote pedestrians using NW 54th Terrace as the route to Wiles Road.
66. **(Tier 1)** Sheet LP-34: A tree is proposed within the 15' utility easement on the west side of sheet, please relocate.
Response: Live oak has been shifted outside of the 15' utility easement.
67. **(Tier 1)** Sheets SP-32: Comment has not been addressed, multiple cross-walks and ramps are missing along the east side of NW 54th Terrace. These crosswalks need to be installed as part of the proposed sidewalk and ramp enhancements.
RESPONSE: The ramps and tactile warnings have been added to the crossings. These crossings on Block 12 may change pending the City's design for that parcel.
68. **(Tier 1)** Drainage layout and profile does not match the cross section. The proposed drainage line and associated catch basins appear to be in the middle of the crowned road, whereas the road design cross section shows curb and gutter drainage.
RESPONSE: The cross section has been revised to place the drainage in the middle of the road.

69. **(Tier 1)** Sheets SP-33: Existing City's forced wastewater main still not shown, please refer to City's GIS system to locate all existing missing City utilities.
RESPONSE: The force main is shown on the plan.

NW 54th Ave:

70. **(Tier 1)** Sheet SP-34: The wastewater connection to Block 9 needs to tie into the existing MAS located within the median.
RESPONSE: The sewer connection is to the existing manhole.
71. **(Tier 1)** CS V1-V1: Lime-rock curb pad for the valley guttering are not shown.
RESPONSE: The lime rock is shown under the valley gutter.
72. **(Tier 1)** CS V1-V1: Annotate the width of the d curb.
RESPONSE: The width of the curb has been dimensioned.

LANDSCAPING

1. General Note: Tree Disposition plans and mitigation will be required to be approved prior to site plan approval for the individual blocks, roadways, and greenway/parks submittals. Comment remains as no Tree Disposition plans or mitigation has been submitted.
Response: Acknowledged. Tree disposition plans have been submitted for review.
2. General Note: Based on the latest review and staff discussions with the applicant regarding the PMDD submittal, any revisions that occurred during that review process will need to be reflected in this site plan submittal. Comment remains until the PMDD has been finalized and any required revisions are made.
Response: acknowledged.
3. Comment not addressed. Include in the site plan legends, the stamped asphalt and permeable paver hatch patterns. Patterns not included in legend as stated in response letter. Hatch and stamped asphalt not provided in legend on site plans. Sheet SP-3 calls out stamped asphalt for crosswalks and Mid-Block detail on sheet RD-4 specifies stamped asphalt.
Response: A site plan legend has been provided on all Landscape sheets to show the typical location for permeable paver and pedestrian crosswalk locations. See civil plan for details.
4. Comment not addressed. Provide paver edge restraint call out on site plans and hardscape plans. Include detail in permit set. Provide call out/label for paver edge restraint on site plans and landscape plans.
Response: per the engineering department request, the multi-use sidewalk and bike lane will be concrete. The concrete sidewalk will act as an edge restraint adjacent to the permeable paver and landscape areas.
5. Comment not addressed. Landscape sheet numbers are to match the Site Plan sheet numbers.
Response: Acknowledged.

6. On Sheet SP-30, review sidewalk connections to existing sidewalk along the east side of Monarch Station and south side of Block 4. Linework is not consistent.

Response: The linework has been corrected.

7. Comment not addressed. Provide root barrier at watermain crossing between Sabal Palm grouping on the north side of 40th Street on sheet LP-7.

Response: a root barrier has been added between the sabal palm grouping and watermain.

8. Landscape treatment at the ends of the medians within the project may need to be revised pending transportation's determination regarding clear sight lines at intersections. Comment remains until determination has been made.

Response: Acknowledged.

9. Street trees required along west side of Banks Road ROW on sheet LP-10. Limit of Work line to include entire ROW. Comment remains. Limit of work under discussion with City and Applicant regarding responsibility.

Response: We have provided landscaping outside of the scope of work line to show that the proposed landscape will meet the current landscape code requirements. The landscape in these areas has been included in our table calculations, however, the installation and management will be at the adjacent property owners responsibility.

10. Comment not addressed; conflict remains. Proposed Veitchia palm in cut-out across from the Block 6 entrance is in conflict with the storm line on sheet LP-11. What is depth of proposed storm line? Discuss with reviewer.

Response: the veitchia palm has been removed as this water line cannot be relocated, and an additional palm has been located in the lake greenspace to meet the requirement.

11. Comment remains. Landscape requirements are to be provided on the east side of Banks Road on sheets LP-14 and 15 and are to be consistent with the west side landscape. Revise limit of work line to include entire ROW width. Limit of work under discussion with City and Applicant regarding responsibility.

Response: We have provided landscaping outside of the scope of work line to show that the proposed landscape will meet the current landscape code requirements. The landscape in these areas has been included in our table calculations, however, the installation and management will be at the adjacent property owner's responsibility.

12. Comment not addressed. Review parallel handicap spaces layout on sheet LP-20 as it appears that it doesn't function properly.

Response: Please see site plan sheet SP-25 for handicap stall layout.

13. For the mid-block crossing on sheet LP-21, recommend providing additional plantings to enhance the pedestrian area and provide a visual cue for the vehicular traffic.

Response: additional landscape shrub material has been provided. Trees cannot be proposed in this location due to utility and drain easements. See sheet LP-21 and LP-38 for the plant list.

14. Shift proposed root barrier locations closer to hardscape and utility locations to allow for more root growth.

Response: root barriers have been moved closer to the hardscape and utility locations.

15. Located at the SE corner of Cullum and 54th, there is a dashed box where 6 Alexander Palms are proposed on sheet LP-27. Confirm dashed box is not an easement. If it is an easement, relocate

palms and label easement as such. Comment remains until coordination with FPL has relocated proposed transformer.

Response: The 10 x 10 transformer easements are temporary placeholders for future FPL transformers. I have adjusted our landscape accordingly at this location.

16. On sheet LP-29 and LP-30, provide location of guardrails. It appears linework is missing and label location is incorrect.

Response: the guardrails have been added and labeled.

17. Confirm proposed Hibiscus at the access opening on sheet LP-30 will not cause any sight visibility issues. Recommend shifting proposed Hibiscus further from access opening to allow clear sight visibility at intersections

Response: the hibiscus has been shifted outside of the sight triangle to allow for clear sight visibility.

18. On sheet LP-30, 31, and 32 streetscape requirements along canal on south side of Cullum are not being met as established in Street Type C-1. Comment remains, further discussion required with City and Applicant.

Response: Cullum road has been modified to allow for street tree locations, and to slow down vehicles by adding medians.

19. On sheet LP-33, landscape island between parallel spaces is required to have one shade/canopy tree. Increase island size by adjusting parking layout or propose structural soil to provide the minimum required soil volume.

Response: The landscape island has been modified to meet our tree and terminal island requirements.

20. Comment not addressed. Verify sheet numbers with pdf file names. Two LP-34 sheets, one for 54th Terrace and the other for plant schedule and details were submitted, as well as two LP-35 sheets with two requirement tables. Submitted PDF file names do not match sheet numbers.

Response: We added sheets to the set to accommodate the additional Street 54th Terrace for resubmission. The correct Sheet LP-34 is for 54th Terrace, and the plant schedule can be located on sheet LP-38. The correct Sheet LP-35 is for 54th Terrace, and the Requirement Tables are located on sheet LP-39.

21. Additional planting around the lift station, street trees and shrubs to be provided on sheet LP-35 once landscape requirements are established within the PMDD guidelines. Provide limits of adjacent block site plans. Comment remains, further discussion required with City and Applicant.

Response: Block 15 site plan has been added. Per the engineering dept – we cannot plant trees or palms in this area. Because of easements and underground utilities, we can only provide a visual screen from 54th Terrace.

22. Comment not addressed. Label and dimension all easements on sheet LP-35.

Response: we have added dimension labels to all easements.

23. Comment not addressed. Provide minimum caliper for proposed trees within the plant list.

Response: we added a column for caliper inches with the proposed trees. See sheet LP-38.

24. For proposed seasonal planting, eliminate 4" spread option. Provide 6" minimum spread to meet the minimum 50% planting coverage at time of installation.

Response: plant spacing has been modified. See sheet LP-38.

25. On sheet LP-39, revise Miscellaneous note and General note stating planting soil depth to be a minimum of 18" for shrubs, hedges, and groundcovers. Revise shrub planting detail on sheet LP-38 accordingly.
Response: note has been modified on sheet LP-38.
26. Revise note regarding the use of Cypress mulch from "strongly discouraged" to "not allowed".
Response: note has been modified on sheet LP-38.
27. Revise native requirement table on sheet LP-3 to include all proposed shrubs, hedges, and groundcovers. Proposed total is 83,473 and the provided native quantities do not meet the minimum 50%. In addition, remove "Excl. Groundcover" text from table.
Response: calculation tables have been updated. Per previous landscape code from October 2023, native shrub calculations excluded groundcover towards the total landscape requirement. As such, we have kept the tabular data to match the previously approved code.
28. Revise plant diversification calculations for Trees and Palms. Provided percentages are not correct per the totals listed.
Response: Tree and palm calculations have been updated. See sheet LP-38.
29. On sheet LP-12, SW corner of intersection on Banks Road, provide note regarding required island tree within adjacent park.
Response: We are proposing the Royal palm adjacent to the terminal island towards the requirements. See sheet LP-39 for tabular chart.
30. Provide note under tables regarding FPL easement restrictions (Max. 14' trees at maturity and Right Tree-Right Place) that limit the use of required shade trees.
Response: Note has been added to the Banks Road and NW 48th Ave. charts. See sheet LP-39.
31. Proposed parking islands under Banks Road Street Type A, NW 48th Avenue Street Type B, and Village Square Street Type B do not meet the minimum parking island area proposed in the PMDD. Street Type A requires a minimum of 250 SF and Street Type B requires a minimum 140 SF.
Response: Parking islands have been modified to provide a minimum 140 s.f. of landscape area and 250 s.f. for associated street types.
32. Fill in missing numbers for "X" under NW 48th Avenue, Street Type B2, table 1.
Response: Chart has been updated. See sheet LP-39.
33. Per the PMDD, the allowed increase in proposed canopy coverage is based on the placement of trees within larger open areas that will allow for the larger mature canopy. The smaller canopy coverage square footage is to be used for trees placed within smaller landscape areas, parking lot islands, and or medians. Correct provided square footages under Green Plan Goal table. See Sustainability for additional comments.
Response: We have calculated category 1 and 2 trees with a higher canopy square footage in areas that are greater than 355 s.f. per IFAS tree standards. When we do not meet the minimum 355 s.f. we are calculating the canopy square footage at the smaller calculation. The calculation table has been split into two sections to reflect this on sheet LP-39.
34. Per PMDD development standards under review, the required north side palms and trees are to be provided within adjacent parcel buffer (High School Parcel) and adjacent greenway (City Civic Parcel). Include north side in calculations. Comment remains. Further discussion with City and Applicant.

Response: we have provided landscape material on the south side of the high school parcel. See sheets LP-31 and LP-32.

35. Section of NW 54th Terrace on sheet LP-35 adjacent to Lift Station. Provide requirement tabulations pending review of the PMDD development standards. Comment remains. Further discussion required between City and applicant regarding requirements along NW 54th Terrace and adjacent lift station.

Response: A landscape requirement table has been included for NW 54th Terrace on sheet LP-39.

36. Include CHR, Red Tip Cocoplum in Plant Key provided on the landscape plans.

Response: Red Tip Cocoplum has been added to the Plant Key.

37. LP-2, correct plant call out label for proposed Date Palms on the east side of the intersection.

Response: plant call out has been revised.

38. Sheet LP-6, landscape does not provide the required clear zone for the fire hydrant west of Park C1.B.

Response: the landscape has been revised to provide a clear zone for the fire hydrant.

39. Sheets LP-8 through LP-10 & sheets LP-16 through LP-18, landscape provided by adjacent property Owners. Comment remains. Limit of work under discussion with City and Applicant regarding responsibility.

Response: Acknowledged.

40. Sheet LP-8, East side of Banks Road, there appears to be existing fence or guardrail linework cutting through landscape area that needs to be removed.

Response: The existing fence has been removed.

41. Sheet LP-9, adjacent to Block 16, shift proposed light pole location to allow Live Oak to be centered in parking island.

Response: light pole has been shifted.

42. Sheet LP-11, provide light pole clearance radius for the poles on the east and west sides of Banks Road within the FPL area.

Response: light pole clearance radius has been added.

43. Sheets LP-13 through LP-15, provide connecting lines for proposed trees of the same species that are under the same plant call out label.

Response: Tree links have been added to the sheets.

44. Sheet LP-14, proposed tree is within 15' of light pole located on east end of Banks Road, closest to sheet LP-15 match line. Shift tree north out of 15' radius.

Response: Tree has been moved out of the light pole radius.

45. Sheet LP-15, groundcover planting labeled as sod and Perineal Peanut. Resolve discrepancy.

Response: the sod label has been removed.

46. Sheet LP-15, north end of Banks Road. Per PMDD this is listed as a street type A1 which requires on street parking. Further discussion required with City and Applicant.

Response: parallel parking has been provided along Block 15 to meet the street type A1 requirements.

47. On Sheet LP-17, verify proposed Live Oaks are spaced far enough apart to allow for fire truck ingress/egress through the Monarch Station fire access route.
Response: we have provided 40' between the live oaks to provide access, and have added a dimension on sheet LP-17.
48. Sheet LP-17, light poles are located at ROW line behind proposed canopy street trees. Further discussion required with City and Applicant regarding the streetscape configuration and cross-section.
Response: light poles have been shifted to City Market to show required street lighting.
49. Sheet LP-21, proposed light poles within FPL area conflict with storm lines.
Response: light pole has been shifted outside of the storm lines.
50. Sheet LP-29, designate shifted location of required island shade tree for southeast bulb out of intersection due to underground canal pipe.
Response: a tree has been added to the southern landscape strip to meet our terminal island requirement. See sheet LP-39 for table.
51. Sheets LP-30 through LP-32, narrow planting on both sides. Street type C1, where is required tree plantings being proposed? Further discussion required with City and Applicant. – HSQ/BETH
52. Sheet LP-36, provide 40' sight triangles with clear trunk height not at intersection.
Response: 40' sight triangle note has been added to the intersection on village green and cullum road.
53. Proposed Crape Myrtle on sheet LP-36 does not allow for adequate separation from storm line. Adjust utility location.
Response: Verawood has been shifted away from storm line with root barrier.
54. On Sheet LP-36 and LP-37, for street type B-3, parking islands are required to provide 1 shade tree per island. Revise proposed species and increase parking islands to meet the minimum required planting area. – HSQ
Response: parking islands have been modified to accommodate a minimum 140 s.f. planting area.
55. Additional comments may be provided upon review of the site plan re-submittal.
Response: Acknowledged.

PHOTOMETRICS

General

1. The photometric plans for NW 48th Ave indicate several spots adjacent to the wetland area where the footcandles at the edge of the roadway are as high as 2.1fc. Field checks will be performed to ensure the light does not trespass into the wetland where it would affect wildlife.
RESPONSE: The wetlands is 100' away from NW 48th Avenue. The buffer adjacent to NW 48th Avenue may have some light spillage, but that does not impact the wildlife in the wetland preserve area.

POLICE

- Recommend seat walls be equipped with anti-skateboard devices.

RESPONSE: The seat walls will have anti-skateboard devices.

- Recommend benches be equipped with a full armrest in the middle to discourage prolonged usage as sleeping accommodations.

RESPONSE: The final bench design will include armrest to discourage sleeping on the benches.

SUSTAINABILITY

1. Sheet SPM-1 identifies 329 parallel parking stalls and 5 EV chargers. Using the agreed requirement of 10% of parking to be EV ready, the number of chargers should be 33. Discussion of the discrepancy is warranted.

RESPONSE: Additional EV-ready spaces are available.

2. In the sustainability checklist, noted under water technologies, the irrigation is temporary to establish the material only. This is contrary to the landscape code and warrants further discussion.

RESPONSE: A permanent irrigation system using the re-use water will be used for the R/W landscaping.

TRANSPORTATION

1. Cullum Road Comment No. 10 appears to remain unsatisfied. Sheet SP-9. City Staff has encouraged the design team to reconsider the need, based on projected traffic volume, for a dedicated EB thru lane. Moreover, the interior roadway network within MainStreet does not have dedicated on-street bike lanes. The existing conditions at this intersection remains unsignalized with a proposed MainStreet shared use pathway that travels along Lyons Road. Bicycle connectivity to the greenway/shared use pathway would allow users to travel north or south along Lyons Road to the nearest signalized intersection. Thus, the removal of two (2) on-street bike lanes (5-foot per bike lane and the dedicated EB thru lane (11-foot) would allow for the design team to install a significant median island/traffic separator along Cullum Road at Lyons Road. A potential 20-foot wide median with a specimen type tree or marque monument sign may serve MainStreet better than bike lanes to nowhere or unwarranted EB thru lanes.

RESPONSE: A median has been added to the Cullum Road intersection at Lyons per our discussions with the city. The bike lanes connect to the Lyons Road on street bike lanes. Bicyclists can use the multimodal path as well and cross at the cross walk if they choose too.

2. Cullum Road Comment No. 11 appears to remain unsatisfied.

RESPONSE: The median has been added to the design.

3. In addition, the PD should be provided with a unique access code as a backup to the Click-2-Enter system.

RESPONSE: All roads are public and do not have gated access.



4. Banks Road Comment No. 14 appears to remain unresolved.
RESPONSE: No R/W on the East side of Banks, North of sample. When the adjacent property owner develops the corner, they can dedicate R/W to improve the intersection and bike lane.
5. Banks Road Comment No. 18 appears to remain unresolved.
RESPONSE: it is resolved with the bike lane per MUTCD and FDOT standard design methodology to access Wiles Road bike lane network. Mandating the bicyclist has to use the sidewalk to access the Wiles Road bike lanes is confusing.
6. NW 40th Street Comment No. 21 appears to remain unresolved. Removal of the WB left turn lane has not occurred.
RESPONSE: The turn lane to the commercial parcel 3 has been removed and the lanes reduced to slow down traffic and back up to the main intersection during rush hours per the city staff mandate.
7. NW 40th Street Comment No. 22 appears to remain unresolved.
RESPONSE: The radius has been reduced as requested. Any large trucks or fire trucks can drive over the curb if needed.
8. NW 40th Street Comment No. 23 appears to remain unresolved.
RESPONSE: The left turn lane has been removed as required by City staff. The left turn movement meets proper turning radius.
9. NW 48th Avenue Comment No. 25 appears to remain unresolved.
RESPONSE: The curb radius has been reduced per city mandate. Large trucks and fire trucks can drive over the curb and sidewalk if needed.

10. City Market Avenue Comment No. 27. Can the petitioner obtain conceptual approval from FDOT regarding the design and connectivity of City Market Avenue onto Sample Road?
RESPONSE: A meeting with FDOT is scheduled for May 22nd to obtain conceptual approval.
11. Additional comments may be forthcoming.
RESPONSE: Acknowledged.

URBAN DESIGN AND DEVELOPMENT

General

1. Roadways plan subject to City Commission approval for vesting of property entitlements.
RESPONSE: Please place the Roadway plans on the Commission agenda for approval.
2. Additional comments may be provided upon review of any revised plans or documents.
RESPONSE: comment acknowledged.
3. Pending reimbursement to the City for professional review services prior to permit issuance.
RESPONSE: Please send the invoice to Rick Stephano at GRE for payment.
4. Pending recorded public access easements.
RESPONSE: The roadways will be dedicated as part of the plat as public R/W and will not require access easements.
5. Pending plat recording.
RESPONSE: Plat is in process for approval and recordation.
6. Pending applicant PowerPoint presentation at public meetings.
RESPONSE: The PowerPoint presentation will be provided a few days prior to public meetings.
7. Pending receipt of 1 digital and printed application packages as previously discussed.
RESPONSE: the packages will be delivered prior to the board meetings.
8. Pending submittal of proposed Amenities/Imaging Package including street signage for review.
RESPONSE: This will be provided to the city separately from this application.

Plan Comments

9. Previous comment #16: It is understood that the roadway plan is for “this specific approval” and that “separate site plans are coordinated internally for consistency and connectivity”. Staff again requests that if an element on the roadway plan is specifically connected to an individual block, for example, guest parking on the residential street for Blocks 5 & 6, that it be identified on the roadway plan. This can be accomplished by simply adding a “G”. Refer to Blocks 5 & 6 DRC comments for specific guidance for the assignment of guest and public parking along this road PRIOR to adding notes to roadways plan.

RESPONSE: All the parking on the public streets is guest parking and are not associated with an individual unit.

10. Previous comment #17c: Ensure light poles do not conflict with parking. The “OK” response does not address the comment.

- Provide a detail depicting a parked vehicle at an on-street parallel parking area.
- Show a light pole and “open” car door.
- Ensure rider may exit the vehicle onto the paver landing without hitting the light pole.

RESPONSE: The light pole is not in the paver area. It sits 4' behind the curb. See plan view for locations.

11. Previous comment #19: Sheet 031-SP-2:

- Only one of the two notes on south side of Cullum Road incorrectly identifying site as Seminole Casino was corrected by applicant. Correct the other note.

RESPONSE: The note has been changed to Mercedes Benz.

12. Previous comment #21: Shts 042-SP-12, 049-SP-19, 064-SP-33, 182-LP-2, 189-LP-10 & 190-LP-11:

- Provide detail for proposed endwall as below, as / if applicable.
- Show wall height above grade.
- Show ground equipment.
- Show what remedies are proposed to conceal the endwall if it projects above grade.

RESPONSE: The end walls are only 7” above grade at the edge of water. Only visible if walking along lake bank.

13. Previous comment #22: Sheet 190-LP-11:

- Sheet 042-SP-13 shows parking in front of lift station. Sheet 190-LP-11 shows sod and trees.
- Revise plans accordingly.

RESPONSE: The area in front of the lift station is paved for maintenance vehicles only. The sod and trees have been removed.

14. Previous comment #25: Sheets 064-SP-33 & 072-FT-1:

- Guard rail on west side of road, on west side of existing lift station to be “decorative”. Add note.

RESPONSE: The note has been added to the guard rail.

15. Previous comment: Per the DRI, the Main Plaza shall be dedicated to the public and restricted for the exclusive use as a park, playground or other recreational purpose. The 20' UE, storm drain and catch basin bi-secting the Main Plaza does not meet the DRI obligation. Revise all plans.

RESPONSE: Per our discussion with City staff we have moved the drainage inlets out of the middle of the park area.

16. Ensure all sheets depicting future fire ingress/egress connection from Monarch Station match. Pending final review from Fire and Landscaping.

RESPONSE: Please see revised plans with the fire access to Monarch Station.

17. SP-10: Address how bike-lane surface will transition from asphalt to raised sidewalks.

RESPONSE: The two surfaces are flush at the asphalt/concrete connection. The 6” rise is done over 20:1 slope 5%.

18. SP-16: Discuss proposed striping versus providing a concrete separator.
RESPONSE: A concrete separator is used near the Wiles Road intersection. The median striping is only used for the narrow lane transition area.
19. SP-19: Clarify why a bike lane is not proposed in roadway design or crosswalk adjacent to City property.
RESPONSE: A bike lane has been added adjacent to the city property by others.
20. SP-21: Crosswalk shown at 15' wide. Revise discrepancy.
RESPONSE: The crosswalk has been reduced to 12' wide.
21. SP-30: Double check line work at Monarch Station Block 4 sidewalk connections.
RESPONSE: The sidewalk connection to Monarch station and the greenway on Block 4 have been modified to allow for pedestrian connection.
22. SP-37: Remove striping at NE corner of Village Square and provide curbing instead in a contoured manner to provide landscaping in lieu of striping.
RESPONSE: The pavement corner has been re-designed as requested.
23. RD-4: Revise details to narrow multi-use path to 10' to allow for more landscaping as discussed.
RESPONSE: the sidewalk has been narrowed at the bulb islands to provide the required landscape green areas for the tree root to grow.
24. Add dimension on site plans depicting 10' and 12' or wider multi-use path locations.
RESPONSE: Dimensions have been added to the pathways.
25. Pending further discussion regarding design of City Market Avenue.
RESPONSE: Meetings with City staff determined the roadway to be constructed as part of this project, but on street parking, utilities and sidewalks will be added later when the adjacent property owner develops the property to coordinate the final layout of the roadway.
26. Pending further discussion regarding design and crosswalk of Cullum Road.
RESPONSE: The current design submitted is based upon the input from the city. This may change in the future pending the signalization at Lyons Road intersection. Per the PMDD guidelines, changes to the roadway can be done during the permitting process if needed.
27. Additional comments may be provided upon review of any revised plans or documents.
RESPONSE: Acknowledged.
28. Additional comments forthcoming on parks plan review regarding utilities within green spaces
RESPONSE: Acknowledged.

Thank you for your consideration.

Very truly yours,
HSQ GROUP, LLC

A handwritten signature in black ink that reads "Jay Huebner". The signature is written in a cursive style with a large, prominent "J" and "H".

Jay Huebner, PE, A.I.C.P., LEED AP



March 29, 2023

CITY OF COCONUT CREEK

Department of Sustainable Development
4800 West Copans Road
Coconut Creek, FL 33063
Liz Aguiar

Project Name: MainStreet Roadways
Project Number: PZ-22120002
Location: Cullum Rd and Banks Road
Review/Application: PMDD Site Plan

Ms. Aguiar:

We have reviewed the comments dated February 6, 2023, and our responses are as follows:

BUILDING

This review shall not imply full compliance with the Florida Building Code. Submittal of a building permit application and plans are required for full review to obtain a building permit.

Note: Every effort has been made to identify code violations. Any oversight by the reviewer shall not be considered as authority to violate, set aside, cancel or alter applicable codes or ordinances. The plan review and permit issuance shall not be considered a warranty or guarantee. The designer is responsible for following all applicable federal, state, and municipal codes and ordinances.

ENGINEERING

General comments:

1. Please provide a full set of site plans. Multiple sheets are missing.
RESPONSE: The original submittal only included phase one of the overall Mainstreet development. The resubmittal includes all the roadways for Mainstreet.
2. Please specify the number and size of the proposed conduit banks under the sidewalks. Coordination with communication companies is required to ensure size, location, and number of conduits are adequate for their needs.
RESPONSE: The coordination with the utility companies is on-going.
3. Clarify if there are any electrical distribution (FPL) lines proposed within the roadway sections.
RESPONSE: Coordination with FPL is ongoing. We placed five 3" conduits in the Right-of-way under the sidewalks that could be used for any dry utility company. Specific areas have been provided in behind the sidewalks for utility equipment.
4. Provide utility easements adjacent to the roadways wherever possible.
RESPONSE: Utility easements are provided in common areas.

5. Coordination with all future utilities is required to ensure there are no conflicts with proposed utility boxes, hand holes, equipment, etc.
RESPONSE: The coordination with utility companies is on going at this time. We have determined the general locations of the utility lines and equipment locations within the common areas behind the sidewalks. See attached site plans for locations of utility equipment and easements.
6. Provide irrigation plans and booster pump station locations with filtration systems.
RESPONSE: Irrigation plans will be provided during the building permit process. The booster pumps for the roadways will be in the parks to service the roadway landscape areas.
7. All existing guardrails must be replaced to meet current standards.
RESPONSE: The existing guard rails will be replaced with new cable style guard rails.
8. Ensure all clear zones for roadways meet City's Standards.
RESPONSE: All above ground elements are at least 2' behind the curb.
9. Ensure there are no hand holds, vaults, pull boxes, and light poles installed on any proposed sidewalks.
RESPONSE: The conduits are under the sidewalk, but the hand holds, vaults and pull boxes are located in easements behind the sidewalk.
10. Ensure all proposed sidewalks comply with ADA standards.
RESPONSE: The sidewalks meet ADA standards.
11. For all proposed cross sections, show the LBR requirement of the 12" compacted subgrade under the sidewalks (min LBR 40).
RESPONSE: The cross sections have been revised to include the 12" stabilized subgrade under the sidewalks.
12. For all proposed cross sections, show transverse slopes of minimum 2.0% for roadways, 1.0% for parking areas.
RESPONSE: The slopes have been added to the cross sections.
13. Specify the material type of the proposed underground utilities on all cross sections.
RESPONSE: The pipe material has been added to the underground utility descriptions.
14. The R.O.W width must match the R.O.W width for each type of road (A, B, C, D, and F) specified in the MainStreet Standard Details. The R.O.W dedication on some sheets does not comply.
RESPONSE: The R.O.W. widths have been revised as needed on the new cross sections to comply with the PMDD typical roadway standard design.
15. All proposed water mains must be at least 12" in size and made of ductile iron pipe (DIP).
RESPONSE: The 12" water mains is only needed on NW 40th Street, Cullum Road and Banks road. The other water mains are sized at 8" diameter which is sufficient for the proposed uses. We want to preserve the option to use PVC -C-900 due to lack of resources for construction materials.
16. All gravity sanitary sewer pipes must be PVC SDR-26, not C-900 as proposed.
RESPONSE: The gravity sewer has been revised to SDR-26.
17. Ensure all proposed utility locations shown on the cross-sections match locations shown on the site plan.
RESPONSE: The cross sections are for general use and in some cases the diameters of the drainage pipes vary depending on the exact location in the roadway.
18. Provide a set of pavement, marking, and signage plans.
RESPONSE: A master pavement marking and signage plan is included with the plans. Please see sheet PM-1.
19. Specify which safety measures will be installed for midblock crossings.

RESPONSE: The crosswalks are raised 1.5" above the adjacent pavement and flashing pedestrian cross walk signs are provided in both directions. See details on the master pavement marking plan.

20. On cross-sections, correctly symbolize the lane control sign for the turning and continue straight through arrows.

RESPONSE: The cross sections have been corrected.

21. Provide a separate set of drainage plans with details of all elevations and drainage flow.

RESPONSE: The drainage plans will be provided at permitting phase. The plans have been submitted to the city in the past for review and is currently in permitting with Cocomar Drainage District.

22. Landscape, lighting, and/or irrigation shall not conflict with and shall maintain 10 feet preferred (minimum 6 feet) of separation from existing and proposed water, wastewater, and drainage.

RESPONSE: Coordination of the separation of utilities with landscaping has been done. Sod is allowed over utilities without separation or mitigation.

23. All fire hydrant clear zones shall be free of landscaping (except sod), mailboxes, parking, lamp-posts and all other objects.

RESPONSE: The fire hydrant clear zones do not have any above ground elements within it.

24. Ensure trees do not obstruct clear sight triangle at stop sign intersections. Please show sight triangles on plans.

RESPONSE: The sight triangles generally cover the sidewalks at the intersections providing a natural clear area with no trees.

25. All proposed details must be City of Coconut Creek's Standard Details, not FDOT as proposed.

RESPONSE: The notes have been revised to reflect Coconut Creek standards instead of FDOT.

26. Please show the subgrade requirements per City of Coconut Creek Standards for all proposed Concrete Valley Gutters, Type F Curbs, and sidewalks.

RESPONSE: The details have been revised to meet City standards.

27. Provide limerock base information under all proposed curbs for the cross sections.

RESPONSE: The limerock information has been added to the cross sections.

28. Roadways abutting county/state R.O.Ws, must obtain approval from corresponding agency.

RESPONSE: Permits from the County and FDOT will be obtained during the permitting process.

29. Weep holes within manholes/catch basins are not permitted.

RESPONSE: The weep holes have been removed from the drainage structure detail.

30. Provide handrails for sections of the sidewalk where drop conditions are experienced.

RESPONSE: This condition does not exist on the proposed roadways. No need for handrails at this time. If a drop off condition does occur, the handrails will be added at that time.

31. For reclaimed water, proposed C-900 piping must be in pantone purple.

RESPONSE: The color has been added to the specifications.

32. Provide additional valves for the reclaimed water line for isolation purposes (as applicable).

RESPONSE: Additional valves have been added to the overall system.

City Market Ave

33. CS A1-A1: For the proposed bike-path, cross-sections show 6" of rock and 8" of compacted subgrade on the same layer. Please clarify.
RESPONSE: The cross section has been revised. Please see cross section sheets.
34. CS A1-A1: Show the type and LBR requirement for the 6" rock (need to be specified as limerock base with LBR 100) and the 8" compacted subgrade (min LBR 40) under the proposed bike-path.
RESPONSE: The specification has been added to the cross sections.
35. CS A1-A1: Header curbs need to be 18" in width with a compacted subgrade per City Standards details.
RESPONSE: Per our discussion with the City a 6" wide header curb is used between the brick pavers and bike lane.
36. CS A1-A1: Please remove the repetitive word (LBR 40) under the roadway subgrade.
RESPONSE: The language has been modified.
37. CS CM-CM: The cross section does not match the location on the plan at sheet SP-18, it appears to match the one on sheet SP-17, therefore, provide the missing cross section on sheet SP-18.
RESPONSE: The cross sections have been corrected.
38. CS CM-CM: Cross section shows a 12" WM, plans show 8" WM. Please clarify as all main lines must be 12".
RESPONSE: An 8" water main is proposed at this location.
39. CS CM-CM: Show the type and LBR requirement for the 6" rock (need to be specified as limerock base with LBR 100) under the proposed bike-path.
RESPONSE: The cross section has been revised.
40. CS CM-CM: For the drive lane, show the type and LBR requirement for the 8" rock (needs to be specified as limerock base with LBR 100).
RESPONSE: The cross section has been revised.
41. CS CM-CM: Ensure that the 12' UE is vacated for R/W dedication.
RESPONSE: We will continue to work with the adjacent property owner to accomplish this request.
42. CS CM-CM: Please clarify if the proposed sidewalk on the west side is existing or proposed. Explain how this cross-section will match the existing roadway conditions.
RESPONSE: The sidewalk on the West side is proposed. CM-CM has been changed to CM1-CM1 since we added another section. The existing Monarch station sidewalk is another 10' west of the R/W and is not shown on the cross sections.
43. Sheet PH-3: City Market Avenue: Proposed light pole (east side) cannot be installed on top of the proposed Reclaimed Water Main. Revise location and apply appropriate separations to avoid conflicts.
RESPONSE: The light pole has been moved to avoid the reclaim water main
44. Sheet LP-7: Ensure that proposed trees are not impacting the reclaimed water main on the east side of the road.
RESPONSE: The trees have been adjusted to not impact the reclaimed water line along the south side of 40th street.

NW 40th Street

45. The proposed double wastewater force main is no longer required along NW 40th St.
RESPONSE: The force mains have been removed from the roadway based upon the latest design of the lift station and force main connection to State Road 7.
46. Sheet SP-6: Utility easement for the proposed wastewater line is not required when located in the dedicated City R.O.W.
RESPONSE: The utility easement has been removed from the R.O.W.
47. Sheet SP-6: Reclaimed water main valve cannot be placed on curbs.
RESPONSE: The valve has been moved out of the curb.
48. Sheet SP-4: Proposed water meters to serve block 6 need to be within a utility easement and cannot be placed on sidewalks.
RESPONSE: The meters are located behind the sidewalk before the front yard wall.
49. Sheet SP-3: Proposed M.H. #35 cannot conflict with the island curb.
RESPONSE: The manhole has been relocated away from the curb.
50. Sheet SP-2: Existing M.H. is not shown on plans (Banks and NW 40th St).
RESPONSE: The existing sewer main and manhole are now shown on the plans.
51. Sheet SP-1: Existing utilities are not shown, some utility call-outs are not accurate (sizes).
RESPONSE: The existing utilities have been added to the plans.
52. CS A2-A2: (West of NW 48th Ave): Show the type and LBR requirement for the 6" rock (needs to be specified as limerock base with LBR 100) under the proposed bike-path.
RESPONSE: The cross section has been updated.
53. CS A2-A2: (West of NW 48th Ave): Show the LBR requirement for the 12" compacted subgrade under the sidewalk (min LBR 40).
RESPONSE: The subgrade has been extended under the sidewalk.
54. CS A2-A2: (West of Lyons Road): Show the LBR requirement for the 12" compacted subgrade under the sidewalk (min LBR 40).
RESPONSE: The LBR 40 has been added to the subgrade notes.
55. CS A2-A2: (West of Lyons Road): Show the type and LBR requirement for the 6" rock (need to be specified as limerock base with LBR 100) under the proposed bike-path.
RESPONSE: The LBR 100 has been added to the rock under the bike path.
56. CS A2-A2: (West of Lyons Road): on the North side, the proposed conduit bank is in close proximity to the 12" water main. Please have the appropriate horizontal separations (minimum 6 feet).
RESPONSE: The minimum of 6' separation is provided between the conduit and water main.
57. Sheet LP-4: Proposed trees (VM2) must meet separation requirements from the storm drain on the south side of the road, and water main/valve north of the road.
RESPONSE: The main/valve on the north side has been adjusted to avoid conflict with the VM2 palm. The storm line is on the north side of the road, the street trees are 8' to the north of the storm line.

58. Sheet LP-5 and LP-6: Ensure that fire hydrant clear zones (show on plans) are met and are not impacted by the proposed trees.
RESPONSE: The plant material has been adjusted to accommodate the FH clear zones.
59. Sheet LP-6: Proposed trees (QV) must maintain appropriate separations from the proposed city utilities (storm drain north side of the road).
RESPONSE: The street trees have between 6-10' separation. When 6' separation, root barriers have been added.
60. Sheet LP-6: Proposed trees (QV) must maintain appropriate separations from the proposed city utilities (water main north side of the road).
RESPONSE: The street trees have between a 8-10' separation from the water line.
61. Sheet LP-7: Please ensure that the proposed trees (QV) are not impacting the reclaimed water main on the south side of the road.
RESPONSE: The reclaimed waterline has been adjusted to provide the appropriate separation.
62. Sheet LP-7: Ensure that all proposed trees in the median must maintain appropriate separations from proposed city utilities.
RESPONSE: The utility lines and trees have been adjusted to avoid conflict.

Banks Road

63. Site plans for Banks Road are missing. Provide site plans for their corresponding cross sections.
RESPONSE: Banks road has been added to the submittal.
64. CS R4-R4 (North of NW 40th St): Show the LBR requirement for the 12" compacted subgrade under the sidewalk (min LBR 40).
RESPONSE: The subgrade has been extended under the sidewalk.
65. CS R4-R4 (North of NW 40th St): The proposed sidewalk needs to be 6" thick since it's in public area (please correct the notation).
RESPONSE: The sidewalk has been revised to 6" thick.
66. CS R4-R4 (North of NW 40th St): Please annotate a header curb on the east side to match what is proposed on the west side.
RESPONSE: The curb has been revised.
67. CS R1-R1 (South of NW 40th St): Bike path on the west side needs to have at least 2.0" of asphalt type SP-9.5 since its shared with the actual driving lane and separated by striping only.
RESPONSE: The asphalt thickness has been revised to 2" as requested.
68. CS R1-R1 (South of NW 40th St): Turn lane on the west side needs to have at least 2.0" of asphalt type SP-9.5, not 1" as proposed.
RESPONSE: The asphalt thickness has been changed to 2".
69. CS R1-R1 (South of NW 40th St): The proposed conduit bank is in close proximity to the 12" water main. Please have the appropriate horizontal separations.
RESPONSE: The water main has been moved away from the conduit bank.
70. CS R3-R3 (South of NW 40th St): Turn lane on the west side needs to have 8" of limerock base with LBR 100, 12" compacted subgrade (min LBR 40), and 2" of asphalt type SP-9.5.
RESPONSE: The specifications have been added to the cross section.

71. CS R3-R3 (South of NW 40th St): Bike path on the west side needs to have at least 2.0" of asphalt type SP-9.5 since it's shared with the actual driving lane and separated by striping only.
RESPONSE: The asphalt thickness has been changed to 2" thick.
72. CS R3-R3 (South of NW 40th St): Please provide at least 8" of compacted subgrade (Min LBR 40) for the proposed bike path on the west side.
RESPONSE: The subgrade has been extended under the sidewalk.
73. CS B3-B3: Parallel parking space width must be 7' with 2' valley gutter (total 9') not 8' in total as proposed.
RESPONSE: The dimensions have been corrected.
74. CS B3-B3: Provide/identify the 12" compacted subgrade (min LBR 40) under the proposed driving lanes and parallel parking stalls.
RESPONSE: The subgrade specification has been added to the cross section.
75. CS W1-W1 (South of Wiles Road): The east side travel lane must have 2" asphalt type SP-9.5, not 1" as proposed.
RESPONSE: The asphalt thickness has been revised to 2".
76. CS W1-W1 (South of Wiles Road): The proposed bike path on both sides need to have at least 2.0" of asphalt type SP-9.5 with 8" of limerock base, not 6" as proposed since it's shared with the driving lanes.
RESPONSE: The asphalt thickness has been revised as noted. The rock has been changed to 8" thick.
77. CS W1-W1 (South of Wiles Road): The proposed conduit bank on the west side does not meet minimum coverage requirements.
RESPONSE: The actual depth of the conduit will be per the specification. The graphical cross section is for illustration only and does not accurately reflect the depth to scale.

Cullum Road

78. Site plans for Cullum Road are missing. Provide site plans for their corresponding cross sections.
RESPONSE: The Cullum Road plans are included with this submittal.
79. Show all existing utilities on Cullum Road.
RESPONSE: The existing utilities are shown on the plans.
80. CS C1-C1 (East of Banks Road): On the north side, clarify if the sidewalk is proposed or same existing one.
RESPONSE: The sidewalk will be replaced with a new sidewalk.
81. CS C1-C1 (East of Banks Road): Indicate the size of the existing drainage pipe.
RESPONSE: The existing drainage pipe sizes are noted on the plans.
82. CS C1-C1 (East of Banks Road): Proposed conduit bank is in close proximity to the proposed water and reclaimed water mains, please have the appropriate horizontal separations.
RESPONSE: The water main has been removed for Cullum road East of Banks. The reuse main has been moved to a new location.
83. CS A4-A4 (West of Banks Road): Parallel parking space width must be 7' with 2' valley gutter (total 9') not 8' in total as proposed.
RESPONSE: The dimension has been revised to 9' space.
84. CS A4-A4 (West of Banks Road): Show the type and LBR requirement for the 8" rock base (needs to be specified as limerock base with LBR 100) under the proposed drive lanes.
RESPONSE: The specification has been revised as requested.

85. CS A4-A4 (West of Banks Road): The proposed conduit bank on the north side must meet minimum cover requirements, these conduits need to be below city utilities.
RESPONSE: The conduit has been lowered graphically on the cross sections.
86. CS C2-C2 (West of Lyons Road): Please annotate the ends of the cross section to show the orientation of the roadway (North, South, East, and West).
RESPONSE: The cross sections have been revised with directions.
87. CS C2-C2 (West of Lyons Road): On the north side, clarify if the sidewalk is proposed or existing.
RESPONSE: The sidewalk on the North side of the road adjacent to Promenade is existing.
88. CS C2-C2 (West of Lyons Road): Identify the existing drainage line along Cullum Road (north side).
RESPONSE: The existing drainage is called out on the plan views.
89. CS C2-C2 (West of Lyons Road): Provide/identify the 12" compacted subgrade (min LBR 40) under the drive lanes.
RESPONSE: The subgrade specification has been noted on the cross section.
90. CS C2-C2 (West of Lyons Road): Bike path needs to have at least 2" of asphalt type SP-9.5 with 8" limerock base, not 6" as proposed, since it's shared with the driving lanes and separated by striping only.
RESPONSE: The specifications have been revised.
91. CS C2-C2 (West of Lyons Road): On the north side, proposed conduit bank is in close to the proposed reclaimed water mains, please have the appropriate horizontal separations.
RESPONSE: Dimensions between utilities have been added to the cross section.
92. CS C4-C4 (West of NW 54th Ave): Identify the existing drainage line along Cullum Road (north side).
RESPONSE: The existing drainage pipes are shown on the cross section.

NW 54th Ave:

93. Site plans for NW 54th Ave are missing. Provide site plans for their corresponding cross sections.
RESPONSE: NW 54th Ave has been included with this submittal.
94. CS E2-E2 (South of Cullum Rd): Identify the existing drainage line along NW 54th Ave (east side).
RESPONSE: The existing drainage pipes are shown on the cross section and plan view.
95. CS E2-E2 (South of Cullum Rd): Clarify if any existing sidewalks on the east and west side will be removed or replaced.
RESPONSE: The sidewalks on both sides of the street adjacent to Blocks 9 and 10 will be replaced with new sidewalks.
96. CS E2-E2 (South of Cullum Rd): Please clarify reference to white cement.
RESPONSE: The white cement has been removed from the specifications.
97. CS E2-E2 (South of Cullum Rd): On the west side, proposed conduit bank is in close proximity to the proposed reclaimed water mains, please have the appropriate horizontal separations.
RESPONSE: The separation dimensions have been added to the cross section.
98. CS E2-E2 (South of Cullum Rd): On the east side, parallel parking space width must be 7' with 2' valley gutter (total 9') not 8' in total as proposed.
RESPONSE: The parking space dimensions have been corrected.
99. CS E1-E1 (South of Cullum Rd): Identify the existing wastewater main, drainage line, and water main along NW 54th Ave.
RESPONSE: The existing utilities are noted on the plan sheets.

100. CS E1-E1 (South of Cullum Rd): Identify the type and dimension of the curbs on the sides of the buffer area (on the east side).
RESPONSE: The curbs have been identified on the cross section.
101. CS E1-E1 (South of Cullum Rd): On the east side, parallel parking space width must be 7' with 2' valley gutter (total 9') not 8' in total as proposed.
RESPONSE: The parking stall dimension have been revised to 9' wide.
102. CS E1-E1 (South of Cullum Rd): The proposed conduit bank on the west side must meet minimum cover requirements, these conduits need to be below city utilities.
RESPONSE: The conduit has been graphically lowered on the cross section.

NW 48th Ave

103. Site plans for NW 48th Ave show an 8" DIP water main runs along the west side of the road, however, the cross section calls for an 12" DIP water main, please correct accordingly to show a 12" DIP water main.
RESPONSE: The water main is 8" DIP on both the cross section and plan view. We reserve the option to use C-900 PVC if DIP is not available at the time of construction.
104. Identify the location of cross section B1-B1 on the site plan.
RESPONSE: Please see location of B1-B1 on sheet SP-26.
105. CS B1-B1: On the east side, proposed conduit bank is in close proximity to the proposed reclaimed water mains, please have the appropriate horizontal separations.
RESPONSE: The separation of utilities has been dimensioned on the cross section.
106. CS B1-B1: Provide base/subgrade material for the proposed 2.33" buffer area (on the east side).
RESPONSE: Subgrade has been extended under the buffer area.
107. Sheet SP-21: Proposed fire hydrant seems to be in conflict with proposed sidewalk. Revise location.
RESPONSE: The fire hydrant has been moved out of the sidewalk.
108. Sheet PH-2: Proposed light poles (west side) cannot be installed in the middle of the proposed sidewalk, please revise location.
RESPONSE: The light poles have been moved out of the sidewalk.
109. Sheet LP 20, proposed trees cannot impact the proposed drainage line along the intersection (South side).
RESPONSE: The trees and utilities have been adjusted to avoid conflicts.
110. Sheet LP-21, please ensure that fire hydrants clear zone (please show on plans) are met and are not impacted by the proposed trees.
RESPONSE: Trees and shrubs have been adjusted to avoid conflicts.

FIRE

This approval shall not imply full compliance with the Florida Fire Prevention Code. A comprehensive evaluation with a Building Department permit is required.

LANDSCAPING

1. Include in the site plan legends, the stamped asphalt and permeable paver hatch patterns.
RESPONSE: Legends and patterns have been added to each plan.
2. Verify with engineering that permeable pavers are acceptable in the walkway connection between the parallel parking spaces and the sidewalk.
RESPONSE: Pavers have been approved.
3. The location of proposed future EV stations and fire hydrants within the landscape bulb-outs, terminal islands, and cut-outs will conflict with required landscape. Provide additional information on how the proposed infrastructure will work with the landscape. Further discussion is required.
RESPONSE: The locations of FH and EV stations have been coordinated with Civil.
4. Recommend providing additional concrete header curb or an alternative paver restraint where the permeable pavers are adjacent to landscape areas.
RESPONSE: We are using a paver restraint below grade. A 6" header curb is proposed between the pavers and the asphalt bike path.
5. Provide and label location of suspended paver system areas on the site plan and landscape plans. In addition, provide location of root barrier on the landscape plans.
RESPONSE: We are showing root barriers on the plan. Both modular root cells and structural soil are recommended in an urban sidewalk environment where there is little to no exposed soil area to allow for root growth and ensure that trees can grow to maturity.

The proposed street sections provide sufficient surface level soil area and soil volume to allow for healthy tree growth in most locations based on the research published by the Urban Horticulture Institute at Cornell University, which is described herein. As such, the applicant proposes to use structural soil in all locations where there is not sufficient soil volume to sustain proper tree growth based upon the volume of soil needed (in cubic feet) to support the anticipated canopy area (in square feet). For purposes of this calculation, sufficient soil volume is calculated based on the canopy area credit established by the City for a Category 1 shade tree, which is 300 square feet. According to the Urban Horticulture Institute at Cornell University the volume needed is two (2) cubic feet of soil for every square foot of canopy area. Considering the anticipated 300 square foot canopy coverage, the soil volume required to allow for mature growth is 600 cubic feet. A soil depth of three feet (3') will be provided. As such, a 200 square foot planting area is required for proper growth of a canopy tree. In areas where 200 SF of planting area cannot be achieved for a Category 1 shade tree, structural soil will be required as envisioned by the adopted MainStreet Design Standards. Please refer to page ___ of the PMDD, which establishes that structural soil shall be required where two (2) cubic feet of soil volume is not provided per square foot of tree canopy anticipated based on the canopy area in accordance with Appendix 1 of City Code Section 13-448

6. Clarify what label D-117 on sheet SP-4 is calling out and verify it does not conflict with proposed landscape.
RESPONSE: The drainage inlet has been moved to the curb line. The landscaping is outside of the 20' wide drainage easement.
7. Adjust landscape conflicts with utilities located on sheets SP-4, 5, 6, 17, and 18.
RESPONSE: Landscaping has been modified to avoid any conflicts with utilities on all the streets.
8. Is the grayscale reuse line on the east side of City Market Avenue existing? Look at the possibility of shifting line east away from landscape islands.
RESPONSE: The re-use line is proposed and the color has been revised to black.

9. Confirm the use of “White Cement” called out on the cross-sections is correct and confirm with engineering and public works that this is acceptable.
RESPONSE: The white cement specification has been removed from the sidewalk design.
10. Review possible use of edge restraint or border for proposed asphalt bike path adjacent to landscape areas.
RESPONSE: A 6” header curb is proposed between the brick pavers and the asphalt bike lane. The asphalt bike lane edge against grass does not require an edge restraint.
11. Clarify use of proposed grass pavers called out on Section R2-R2 on sheet RD-1.
RESPONSE: The grass pavers has been changed to pervious pavers.
12. All Mainstreet roadways are to be included in the roadway submittal package. Future submittals to include remaining roadways for review.
RESPONSE: Acknowledged.
13. Provide sight triangles for all roadway and driveway intersections.
RESPONSE: Sight triangles are shown on landscape plans.
14. On the landscape plans, show location of future charging stations and how they will be located in relation to required landscape.
RESPONSE: EV stations have been shown on the landscape for 40th street. They are shown adjacent to the Veitchia palm cuts and will provide for (2) chargers at each location.
15. Provide plant list legend on each landscape plan that includes plant id code and common name for reference.
RESPONSE: Plant list legend is provided on each sheet.
16. Include all light pole locations with dashed circles demonstrating required setbacks from trees and palms. Minimum 15’ is required for canopy trees and 7.5’ for small trees and palms.
RESPONSE: The Radius’s have been shown on light poles.
17. Proposed Live Oaks on north side of road, west of 48th Avenue are in conflict with storm line on sheet LP-6.
RESPONSE: Utilities and Trees have been adjusted.
18. Provide and label all existing and proposed easements on the landscape plans.
RESPONSE: Labels have been added to all sheets.
19. Verify that the proposed landscape in the medians at the intersection on sheet LP-6 do not cause any site visibility issues when making the turning movements across the intersection.
RESPONSE: Sight visibility lines are shown, a clear zone from 24” to 6’ have been provided.
20. Provide consistent hatch patterns for proposed shrub material on all the landscape plans. It appears that some are missing from sheets.
RESPONSE: Hatch patterns have been removed from the plans.
21. Proposed Ligustrum and Live Oak on sheet LP-7 are in conflict with the storm line that crosses 40th Street north and south.
RESPONSE: Trees and utility lines have been adjusted to avoid conflicts.
22. For the mid-block crossing on sheet LP-18, look at alternate landscape designs to signify the crossing visually to vehicular traffic.
RESPONSE: Flowering trees and alternate shrub material have been added.
23. Are sheets LP-16 and 17 missing from the submittal? Site plans sheets SP-16 and 17 included in the submittal propose landscape areas with parallel parking.
RESPONSE: LP-16 and 17 are south of our limit of landscape work.

24. On sheet LP-20, proposed Live Oaks are within the sight triangle. Clarify dashed line that runs east-west through the same bulb-out.
RESPONSE: A clear visibility zone from 24" to 6' has been provided. The line is an old base line and has been removed.
25. Identify dashed line on sheet LP-21 that runs east-west north of the proposed fire hydrant.
RESPONSE: The line is an old base line and has been removed.
26. Provide locations for required site amenities and bus stops along the streetscapes per the Mainstreet Design Standards on both the site plans and landscape plans.
RESPONSE: The site amenities are being shown on the greenspace plans. They are outside the ROW. We are producing a separate amenity that is being reviewed with city staff.
27. To help create a sense-of-place and provide diversification within the MainStreet development, consider proposing alternate species on different streets that provide a different visual experience for vehicular and pedestrian traffic. In addition, alternative plant palettes can be used to visually signify entrances to the different residential blocks or important nodes within the development.
RESPONSE: We have adjusted the planting to provide plant and tree diversity.
28. Provide data tables for required landscape within each roadway per the MainStreet Design Standards and approved alternate design solutions in the PMDD document.
RESPONSE: Data tables have been added for each section of roadway.
29. Additional comments may be provided upon review of the site plan re-submittal.
RESPONSE: Acknowledged.

POLICE

1. On plan 33-SP-4 MainStreet NW 40th St – It is not clear what traffic control device will be used at City Market Ave. Will this be controlled by a traffic light or stop sign? PD requests a traffic light with cross walk signals.
RESPONSE: The intersection of NW 40th Street and City Market Avenue will be a 3 way stop condition. A traffic signal is not warranted at this intersection.
2. On plan 46-SP-16 MainStreet City Market – PD requests this be redesigned into roadway that utilizes a traffic signal to allow traffic to enter northbound on City Market Ave from eastbound on Sample Rd and also allows for exiting southbound City Market Ave traffic on to eastbound or westbound on Sample Rd.
RESPONSE: Per our discussions with the City, County and FDOT we are working on a possible signal at this location, but it has to be warranted by the County traffic division. Also, this intersection is not adjacent to our parcels and we agreed to only construct City Market Avenue and right turn lane into the adjacent property.
3. On plan 51-SP-21 MainStreet NW 48th Ave – The two sections where the pedestrian/bike path cross the roadway, utilize some type of crosswalk signal. Both paths are at curves in the roadway that can inhibit field of vision from both vehicles and pedestrians.
RESPONSE: The mid-block cross walks will be raised and pedestrian signage provided in both directions.
4. On plan 52-SP-22 MainStreet NW 48th Ave – PD requests that roundabout be added to the intersection at Cullum Rd to allow for a more natural ease on to the roadway off of 48th Ave and also to assist in reducing the speeds on Cullum Rd.
RESPONSE: Per our discussions with staff, the roundabout is not the right solution for this location and we do not have control over the property to the North to modify the entrance.

5. Ensure all proposed landscaping follow the CPTED principles of 3' for shrubs and 6' up or higher for trees, to prevent obstructions of natural surveillance.
RESPONSE: The proposed shrubs and trees meet CPTED requirements.
6. Proposed LED lighting appears to be adequate, however, will continue to evaluate.
RESPONSE: Thanks.
7. Nothing further at this time, however, we will continue to review the project as it progresses.
RESPONSE: ok.

SUSTAINABILITY

General

1. Refer to zoning comments for:
 - a) Planning and Zoning Board and City Commission presentations.
 - b) Public meeting package submittal requirements.
 - c) Public outreach requirements.**RESPONSE: Please see responses below.**
2. Additional comments may be provided at DRC meeting and/or upon review of revised application.
RESPONSE: Please see responses below.
3. Staff understands that this submittal is not a typical site plan and, therefore, does not have the same ability to meet certain sustainability goals. The comments below consider this unique situation.
RESPONSE: The roadways will not obtain green certification but has been designed to be in harmony with the Mainstreet sustainability goals and standards.
4. For future submittals, affix the Sustainability Checklist to a sheet in the drawing set. A copy of the "green" data sheet is available upon request. This will be used for this submittal in lieu of the FGBC certification. Call Linda Whitman to discuss this process for this submittal.
RESPONSE: The 2021 green sustainability check list has been added to the site plan detail sheets.
5. All Sheets: Revise the Legend so that F EV-2 represents "Future EV Dual Charging Station"
RESPONSE: The EV icon has been updated.
6. Provide a single overall sheet showing all the proposed and future EV stations.
RESPONSE: Please see the pavement marking plan with the EV charging stations located on the plan.
7. Show the EV chargers on the landscape plan to check for conflicts.
RESPONSE: The EV charger locations are shown on the landscape and site plan sheets.

TRANSPORTATION

1. Include a typical roadway section of Cullum Road with this application.
RESPONSE: Cullum roadway design has been coordinated with City staff and is now included within this submittal.
2. Sheet SP-22 shall provide the sidewalk connection from NW 48th Avenue to Lyons Road along the south side of Cullum Road.
RESPONSE: The sidewalk is combined with the Block 1 perimeter buffer sidewalk connecting NW 48th Avenue to Lyons. Please see Cullum plans for details.
3. Sheet SP-16 illustrates the City Market Avenue connection to Sample Road. This improvement is subject to the review and approval from FDOT.
RESPONSE: We are coordinating with the FDOT for the access point on behalf of the City. A pre-application meeting has taken place and a follow up meeting will be conducted once the information requested by FDOT has been submitted for their review.
4. Provide the traffic simulator justification for the need of a northbound left turn lane proposed at Banks Road at NW 40th Street.
RESPONSE: The traffic analysis includes operational analyses of the intersection on Banks Road & 40th Street. Upon project buildout, the traffic projections indicate that the PM peak hour through and right-turning traffic is anticipated to be 77 peak hour vehicles, paired with 40 northbound left-turning vehicles. Given the volume of left-turning traffic projected during the PM peak hour, a separate left-turn lane is determined to be appropriate to separate the left-turn movement from through + right-turning volumes. Furthermore, potential growth in traffic at the Seminole Coconut Creek Casino in the future is anticipated to reinforce the need to separate left turns from through/right-turning vehicles on the northbound approach at the intersection.
5. Proposed mid-block crossings shall be designed using appropriate separation or distances from controlled intersections. Identify all unsignalized mid-block pedestrian crossings, dimension distances from closest intersections and provide the proposed typical mid-block crossing details, pavement marking, signage, warning devices, etc. in compliance with the Manual for Uniform Traffic Control Devices.
RESPONSE: The mid-block location is more than 300 from a controlled stop intersection. The mid-block crossings include raised cross walk with stamped asphalt with white lines. Pedestrian crossing signage is provided before each crossing.
6. Sheet SP-16. Identify the pavement materials anticipated for the proposed 5-foot bike trail vs. the proposed 8-foot sidewalk. How are these delineated as two separate facilities?
RESPONSE: The sidewalk is concrete and the bike lane is asphalt to visibly show the two uses plus the bike lanes have bike lane pavement markings. The asphalt surface is more desirable for bike riding compared to the concrete sidewalk with expansion joints causing an uncomfortable ride.
7. Sheet SP-16. Provide a narrative on how the west side improvements along City Market Avenue are harmonized with the existing shared use pathway adjacent to Monarch Station.
RESPONSE: The sidewalk along City Market Avenue connects to the Monarch Station sidewalk allowing for access to both properties adjacent to City Market Avenue. The section of City Market Avenue South of Blocks 4 and 8 are not under our control. The applicant is willing to work with the city in return for development credits. The final design of this section should be coordinated with the adjacent property owner prior to construction.
8. Sheet SP-1. Provide a public sidewalk along the north side of NW 40th Street east of NW 54th Avenue. If no designated bike lanes or bike trails are provided on NW 40th Street then the minimum sidewalk width shall be no less than 10-feet on NW 40th Street.
RESPONSE: The bike lane and sidewalk are extended to the West end of the project limits. The property to the West of our property does not have the proper Right-of-way dedication to provide space for the sidewalk. Please contact that property owner to dedicate land and build the sidewalk.

9. Sheet SP-2. Remove the on-street parking spaces proposed along NW 40th Street west of Banks Road. Remove the NB left turn lane at Banks Road at NW 40th Street.
RESPONSE: The previous requests for on street parking on the North side of NW 40th Street west of Banks was made and we complied with it. We think this is a good idea for more parking in this area. The left turn lane is required based upon the number of trips at this intersection. See response number 4 above for more information.
10. The SP Legends include Proposed EV Charging Stations, proposed EV Dual Charging Stations, and Future EV Charging Stations. Provide a summary of the tabulations assigned to these locations. Provide an EV Charging Station Typical Detail.
RESPONSE: The legend has been updated to indicate charging stations and future charging station locations. A detail of the proposed type of charger is included in the site plan detail sheets.
11. Sheet SP-3. Identify the route pedestrians would use to cross from Block 8 to the Public Park C1.
RESPONSE: Proposed cross walks are located at Banks and NW 40th and at NW 40th and City Market Avenue.
12. Sheet SP-3. Identify a handrail at the back of the sidewalk or confirm that a drop condition will not be constructed along Block 8 that fronts NW 40th Street.
RESPONSE: A drop off is not proposed at the back of sidewalk on NW 40th Street.
13. Sheet SP-3. Identify the use or facility at the location of the proposed sidewalk connection from Block 8 onto City Market Avenue south of NW 40th Street.
RESPONSE: The mid-block pedestrian crossing located at the South end of Block 8 allows pedestrian connection to the greenway along the South side of Block 4 leading to the commercial uses on Block 3.
14. Provide the location of street lights, decorative lights, pedestrian lights, etc. and the required infrastructure (pull boxes, conduit, etc.).
RESPONSE: The street lighting locations are shown on the photometric plan, site plan and landscape plan. The final electrical design will be provided at a later date. We understand the pull boxes are not allowed in the bike lane or sidewalk and will most likely be behind the light pole in the brick paver area serviced from the conduit bank under the sidewalk.
15. Remove the designed WB left turn lane on NW 40th Street at NW 48th Avenue and reduce the width of the WB thru lane.
RESPONSE: we disagree with this request. The truck traffic to the commercial parcels access at this location and this allows the trucks to stack in the turn lane and not block thru westbound traffic.
16. Sheet SP-7. Provide a plan that will accommodate the future mast arms and traffic signal equipment at the intersection of Lyons Road and NW 40th Street.
RESPONSE: The proposed R/W dedication provides enough room for the mast arms. Possible locations of the master arms have been added to the intersection.
17. Sheet SP-7. Sidewalk widths shall be a minimum of 10-feet if no designated bike lanes are provided on NW 40th Street at Lyons Road.
RESPONSE: The sidewalk has been widened to 10' wide between the Block 3 access and Lyons road.
18. Provide a chicane or similar traffic calming device within a segment of Cullum Road in an area between NW 48th Avenue and Banks Road.
RESPONSE: The Cullum road design has been revised to meander between NW 48th Ave and Banks as a traffic calming method.
19. Rectangular Rapid Flashing Beacons shall be used at all unsignalized pedestrian mid-block crossings and shall include all supportive pavement marking and signage compliant with the Manual of Uniform Traffic Control Devices (MUTCD).
RESPONSE: The beacons are part of the mid-block pedestrian crossing and a raised cross walk. Please see detail in the site plan detail sheets.

20. Additional comments may be forthcoming.
RESPONSE: Additional responses will be provided if necessary.
21. In review of the Cullum Road typical section, why is the proposed water main at the lowest elevation along the north side of Cullum Road? The City's Utilities & Engineering Dept. requests to have the water main at a higher elevation for future maintenance reasons;
RESPONSE: The water main has been removed from Cullum road East of Banks since it is not needed.
22. In review of the Cullum Road typical section, there appears to be a conflict based on the information being depicted within the cross-section detail and what is actually in place. Please refer to the City's GIS map for as-built utility information located on the City's website.
RESPONSE: The existing utilities have been updated as much as possible to match city GIS and survey information provided to locate the utilities on the plans.
23. Both Cullum Road and NW 48th Avenue's roadway drainage configuration shall accommodate the omission of the F-curbs inlets. Provide a narrative response;
RESPONSE: The proposed inlets in the F curbs will be F-curb inlets.
24. It is unclear if the Cocomar open drainage ditch south of Cullum Road will be reshaped and/or re-graded during construction. Please provide a narrative response;
RESPONSE: The ditch will be regraded and cleaned up to function properly. This area also counts towards our 15% lake area requirement.
25. A roadway guardrail is, first and foremost, a safety barrier intended to shield a motorist who has left the roadway. The City is requesting removal of the old guardrail along Cullum Road to accommodate the design and construction of a new safety barrier to be located adjacent to the northern limits of the above referenced drainage ditch. The new barrier location will serve as the protective measure for the all of the users of the shared use pathway as well as the motorist on Cullum Road and thus, this would also allow the City's Public Works Staff to have future vehicle access the landscape buffer during routine maintenance periods;
RESPONSE: Per our meeting with the City we moved the guard rail location to the top of bank and used a cable guard rail design to be more aesthetically pleasing.
26. In review of the Cullum Road typical section, the City requests that the two (2) 4-foot landscape buffers proposed on the south side be combined into one (1) continuous 8-foot or larger buffer at the back of the southern curb. The shared use pathway can be reduced from 12-feet to 10-feet on the south side of Cullum Road. This design change would provide for a curb/gutter segment, 8-foot or more area for a landscape buffer, a 10-foot shared use pathway, a protective barrier, and finally the Cocomar drainage ditch;
RESPONSE: The proposed design provided for Cullum is based upon our meetings with staff to provide the cross section that works for that area with the constraints provided do to R/W width limits and canal maintenance requirements.
27. Provide a typical section for the portions of NW 48th Avenue where there is no on-street parking proposed;
RESPONSE: Please see additional cross section added for NW 48th Avenue.
28. The existing Cullum Road section that connects to Lyons Road has a guardrail that extends along SB Lyons Road. Provide a narrative response on how the design team is addressing this condition with the future shared use pathway which fronts Block 1. The existing safety barrier appears to restrict access for the shared use pathway that shall connect to Lyons Road;
RESPONSE: The guard rail is only needed adjacent to open canal/ ditches or lakes. In front of Block one the canal will be filled in and a drainage pipe connection between the canal in front of the wetlands and the connection to the existing pipe under Lyons road.
29. Adjust the driveway radii and alignment of intersections;
RESPONSE: The roadway alignment at Banks and Cullum has been revised to align with the center line of the roadways.

30. Remove the merge condition on NW 40th Street and extend the median; and
RESPONSE: The design has changed to allow the thru traffic on NW 40th without a merge.
31. Traffic calming measure(s), such as a chicane or other city approved traffic calming devices, shall be incorporated into the segment of Cullum Road between NW 48th Avenue and Banks Road.
RESPONSE: Cullum Road has been revised to include a meandering alignment between NW 48th Ave and Banks road. This has been discussed with city staff for the overall design.

URBAN DESIGN AND DEVELOPMENT

General Comments

1. Vesting of property entitlements is completed by Ordinance with site plan approval. Be advised, the City Commission may place restrictions or development conditions.
RESPONSE: We understand additional conditions could be placed on the project thru the board approval process.
2. Outstanding PMDD comments may impact this roadway site plan. Comments herein may not cover all concerns. Changes to the PMDD may trigger changes to this site plan.
RESPONSE: Coordination between the PMDD design and standards and the site plan design has been done internally with the team consultants. Any changes to the PMDD thru the approval process will be updated on the roadway site plans.
3. The City has retained professional services to conduct landscape review of all Development Review Applications. Per Sec.13-80(b) of the City's Land Development Code, the cost for these services shall be billed to the applicant on a cost recovery basis. *Please acknowledge and provide name and contact information for person(s) responsible for reimbursement to the City.*
RESPONSE: Please send the invoices to Rick Stephano at Rosemurgy properties.
4. Acknowledgements to DRC comments *may* not show compliance. Corrections shall be done through plan revisions. All corrected plans shall be re-submitted per digital submittal requirements. Written responses shall identify appropriate sheet(s) where corrections have been made.
RESPONSE: Please see detailed responses to comments within this letter.
5. A number of outstanding details are missing or not shown in this submittal. Additional comments may be provided at the DRC meeting or upon review of the roadway plan re-submittal.
RESPONSE: Please see additional site amenities and design details on the detail sheets.

Public Meetings

6. Applicant shall be prepared to make a PowerPoint presentation at the Planning and Zoning Board and City Commission hearings including color renderings and aerials of the project, as applicable.
RESPONSE: The PowerPoint presentation will be provided for the board presentations.
7. Prior to Planning and Zoning Board hearing, applicant shall provide one (1) digital copy and thirteen (13) printed sets, individually bound, stapled & 3-hole punched of the following:
- Site plan package;
Note: Digital copy to be **unlocked and unsigned**.
Note: Printed copies to be no larger than 11"x17" in size.
 - PowerPoint presentation;
Note: Printed copies to be no larger than 11"x17" size.
 - Public outreach report as part of larger effort;
 - Sustainable (Green) efforts;
 - Each set of DRC comment/response document:
Note: Printed copies to be 8.5"x11" in size.
- RESPONSE:** The board meeting documents will be provided to the city once city staff has deemed the plans ready for board approval.

Public Outreach

8. Roadway plan amenities and public spaces shall be made part of future public outreach efforts.
RESPONSE: A public outreach program has been developed for all of Mainstreet and will include the site amenities for public participation.

Utilities

9. Pending location of FPL transformers and all other above ground utility boxes. All ground mounted utility boxes shall ensure adequate green space is provided for required screening shrubs and trees to allow for normal growth patterns without conflicts.
RESPONSE: The coordination with utility companies is on going at this time. We allow for a 6' wide utility easement in the common areas for splice boxes and hand holds plus we strategically placed utility equipment in the parks in locations that should work well for the utility company above ground equipment. This is shown on the roadway site plans. The equipment is located in green areas and will have proper landscape screening pending the final utility equipment placed in those locations at a later date.

Overall Site Plan Comments

10. An overall master plan that includes all amenities combined is necessary to determine conflicts between hardscape, landscaping, parking etc. Ensure public roadways include street furnishings including but not limited to pedestrian lighting, benches, trash receptacles, bike racks, bus shelters, etc., consistent with the City Commission approved City Amenities Package. See additional comments below.
RESPONSE: The amenities to include bus stop shelters, trash receptacle's, benches and bike racks are purposely located in the common area parks and outside of the road right-of-way to avoid conflicts with roadway street lights, landscaping and utilities. The intent it to allow active use of the sidewalk and bike lane without interruption of amenities.
11. Roadway light poles have not been shown on roadway plan. Staff is unable to determine conflicts between poles, signage, sidewalks, and trees. See comment above.
RESPONSE: The light pole locations are shown on the roadway site plan and landscape plans.
12. Bus shelters have not been shown on roadway plan. Staff is unable to determine conflicts between bus shelters, light poles, signage, sidewalks, and trees. See comment above.
RESPONSE: The bus shelter locations are within the parks area to avoid conflicts with landscaping, light poles and utilities.
13. Ensure street hardscaping complies with City Amenities Package. Provide details for:
a) Permeable pavers.
b) Stamped asphalt.
c) EV charging station(s).
d) Bus shelters.
RESPONSE: The permeable pavers detail is on the cross-section sheet. The stamped asphalt specification is on the cross-section notes. The EV charging station detail is provided on the detail sheet. Bus shelters are provided in the amenities package.
14. Except for some benches on Sht.SP-20, street furnishings (locations) have not been shown.
RESPONSE: The benches are in the common area parks. This is shown on a separate site plan application for Mainstreet parks.
15. Refer to landscape comments. Ensure irrigation pump locations are shown.
RESPONSE: The irrigation pumps are part of the individual parcels and are in the private parcels. Common area irrigation pumps are located in the parks parcels.
16. Staff recognizes roadway guardrails are intended to first, serve as a safety barrier. However, every effort shall be made to provide a system that can also be attractive, as permitted. Terminology such as to "remain as-is" is not an acceptable design.

RESPONSE: The existing guard rails will be removed and the new guard rails are cable style guard rails at the proposed locations.

17. Provide information for completion of Cullum Road roadway section west of Banks Road. Please note, if a guard rail is necessary, it should be “decorative” and consistent with other sections of Cullum Road east of Banks Road, as discussed.
RESPONSE: Please see attached Cullum road to include West of Banks. The missing link of Cullum road does not require guard rails.
18. Cullum Road north of Block 9 must be included on this roadway plan application. Revised plans shall reflect the comments recently provided by the City for this Block, the “ditch” and Cullum Road streetscape design.
RESPONSE: The ditch along Cullum road adjacent to Block 9 will be filled in and piped. Please see Cullum roadway plans for more details.
19. Sustainable elements: The City expects projects within the MainStreet Project Area to incorporate conspicuous displays of green technology, which can be easily seen by the general public while being an integral part of the project. Refer to Sustainability Comments for further information.
RESPONSE: The on-street parking includes EV charging stations at certain locations to serve the general public. These are highly visible from the roadway and meet the conspicuous displays of green technology requirement. Please see the site plans for locations. The master pavement marking plan has them located on it for an overall site location map. The applicant also proposes solar bus shelters, rainwater harvesting, solar trash receptacles, pervious pavers, solar lighting and solar covered picnic benches.
20. Sheet SP-2 – Dimension utility easement width on north side of NW 40th St.
RESPONSE: The utility easement has been dimensioned as requested.
21. Public Access: Upon final site plan approval by the City Commission, property owner shall record public access easement(s) for all portions of sidewalks, park space, greenways, etc., that shall be located within private property but shall/may be used by the general public. Copies of recorded documentation must be provided to the City prior to building permit issuance. Pending Plat approval and recordation for any additional right-of-way dedication, easements, etc.
RESPONSE: The easements will be submitted to the city during building permit process based upon the final locations as determined on the final approved site plans.

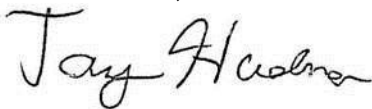
Signage

22. Pending completion of a street signage plan. Street signs throughout the MSPA shall be of a consistent style. Refer to the City Commission approved City Amenities Package. Sign plan should include traffic, directional and wayfinding signage, etc.
RESPONSE: The street signage will be part of the amenities package submitted under separate application.

Thank you for your consideration.

Very truly yours,

HSQ GROUP, LLC



Jay Huebner, PE, AICP